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BEFORE THE PUBLIC UTILITIES COMMISSION OF THE STATE OF CALIFORNIA

A2102010

Application of the California High-Speed Rail
Authority for Approval to Construct One
Underpass Grade-Separated Crossing at 7th
Road (MP 223.96), Under Two Proposed High-
Speed Rail Tracks As Part of the Proposed
High-Speed Rail Grade-Separated Structure,
Hanford Viaduct, Located in the County of
Kings, State of California.

Application No. _____

APPLICATION

In accordance with Rule 3.7 and 3.9 of the Rules of Practice and Procedure of the California Public Utilities Commission (“Commission” or “CPUC”), the Applicant, California High-Speed Rail Authority (“Authority” or “CHSRA”) hereby seeks authority from the Commission to construct one underpass grade-separated crossing at 7th Road at CHSRA’s milepost (MP) 223.96 under two proposed high-speed rail tracks as part of the proposed High-Speed Rail grade-separated structure, Hanford Viaduct, located in Kings County. This project is located within the Construction Package 2-3 (“CP 2-3”) in Kings County, State of California. The proposed underpass grade separation at 7th Road will be a part of the grade-separated structure, Hanford Viaduct as well as a part of the proposed California High-Speed Train System (CHSTS).

In support of this application, the Applicant asserts that:

1. The Applicant’s exact legal name is California High-Speed Rail Authority, hereinafter referred to as “Authority” or “CHSRA”, a public agency and subdivision of State of California formed pursuant to the California High-Speed Rail Act. (California Public Utilities Code § 185000 et seq.)

2. Applicant's principal place of business is located at the office of:

California High-Speed Rail Authority (CHSRA)

770 L Street, Suite 620

Sacramento, CA 95814

Attention: Bruce Armistead - Director of Operations and Maintenance

E-mail: Bruce.armistead@hsr.ca.gov

3. All correspondence, communication notices, orders, and other papers relative to this application should be addressed to:

CP 2-3 Team

California High-Speed Rail Authority

1775 Park Street, Building 75A

Selma, CA 93662

Attention: Haidar Haddadin– Construction Oversight Manager

E-mail: Haidar.Haddadin@hsr.ca.gov

4. CHSRA is the railroad property owner at the proposed crossing. CHSRA is working cooperatively with the stakeholders along the corridor, which include government agencies and railroads.

5. The authorization to construct the Project is requested pursuant to Section 1201 through 1205 of the Public Utilities Code and is made in accordance with Rule 3.7 and Rule 3.9 of the CPUC Rules of Practice and Procedure.

6. The construction of the 7th Road underpass grade separation will include improvement of the existing roadway under the two proposed high-speed rail tracks. This construction will conform to CHSRA, CPUC and AASHTO standards.

The CP 2-3 project limits extend 65.5 miles from East American Avenue in Fresno County to one mile north of Tulare-Kern County line. The CP 2-3 project consists of two CHSTS mainline tracks, generally aligned in a north-south direction. The proposed crossings of the CP 2-3 project include approximately thirty-six (36) grade separations in the counties of Fresno, Tulare, and Kings, including viaducts, underpasses and overpasses.

7. The purpose of this underpass grade separation in the CP 2-3 project is to provide for the safety of motorists and pedestrians. The underpass grade separation will allow for safe and simultaneous movement of roadway and rail modes of travel. All CHSRA structures are in compliance with Arema standards throughout the project.

For the protection of the general public and CHSTS passengers, the CHSTS will be a closed corridor with a continuous 8 foot high welded wire mesh Access Restricted (AR) fence or barrier to prevent unauthorized access to CHSRA right-of-way. In addition to the AR fence or barrier walls, intrusion monitoring and detection is proposed.

8. For the proposed crossing stated herein as required by CPUC Rules of Practice and Procedure, Rule 3.7 and Rule 3.9, the CHSTS milepost, proposed CPUC crossing number, and DOT crossing number on the Sierra Subdivision are listed in the following table. The underpass grade-separated structure will be in coordination with the construction at 7th Road.

Crossing Name	CHSTS Milepost	Proposed CPUC#	Proposed DOT#
7 th Road Underpass	223.96	135S-223.96-B	973186C

9. The nearest existing public crossing on each side of the proposed structure at 7th Road is located as follows: (Rule 3.7 (b))

- At-grade crossing at State Route (SR) 43, San Joaquin Valley Railroad 's (SJVR) crossing, CPUC Crossing Number 103BD-250.00 and DOT Crossing Number 750815V, to the northwest of the proposed crossing, and the 7th Street at-grade crossing, SJVR's crossing, CPUC Crossing Number 103BD-249.00 and DOT Crossing Number 750814N to the northeast of the proposed crossing.

10. The overall length and width dimensions of the proposed 7th Road underpass grade-separated structure is provided in the structural drawings in Exhibit D.

11. The overhead contact system, which will supply energy to the rail vehicles, is consistent with the requirements of General Orders 26-d, 95, 128, and 176.

The horizontal and vertical clearance dimensions provided in the structural drawings in Exhibit D comply with CPUC General Order 26-D. See table A for further information for horizontal and vertical clearances for 7th Road.

Table A: Exhibit D Structure Overhead Drawings

Crossing Name	Minimum Vertical Clearance	Minimum Horizontal Clearance
7 th Road Underpass	27'-5"	16'-0" from centerline of road to closest structure obstruction

For the 7th Road underpass grade separation, the following coordinates are latitude 36.327° and longitude -119.591°.

12. The CHSRA, as the lead agency under the provisions of the California Environmental Quality Act (CEQA), prepared the *Final Environmental Impact Report/ Environmental Impact Statement (Final EIR/EIS)* for the Fresno to Bakersfield section of the California High-Speed Train project. The Federal Railroad Administration (FRA) was the lead agency under the National Environmental Policy Act (NEPA). The Notice of Determination (NOD) for the *Final EIR/EIS* was certified on May 07, 2014, and the Record of Decision (ROD) was dated on June 27, 2014. The *Final EIR/EIS* for the Fresno to Bakersfield Section of the California High-Speed Train project was provided to the Commission (Rule 2.4) on archival and standard CD-ROM/DVD as Exhibit I to Application A1709017. The *Final EIR/EIS* is made a part of hereof by reference. A Notice of Availability for the *Final EIR/EIS* and Certificate of Service are filed with this application.

Construction of the California High Speed Train System (CHSTS) complies with all environmental regulations for the proposed construction of the 7th Road underpass under the high-speed rail tracks.

The environmental impacts and mitigation specifically associated with the portion of the project can be found within the following sections of the Final Environmental Impact Report/Environmental Impact Statement (Final EIR/EIS) for the Fresno to Bakersfield Section of the California High-Speed Train Project, dated April 2014, related to the proposed railroad crossing at California High-Speed Rail Train System, Sierra Subdivision milepost 223.96.

Final EIR/EIS Summary

- Volume I: Report, Chapter 2, Alternatives, discusses the alternative alignments contemplated in the EIR/EIS. (2-21 to 2-32)
- Volume I: Report, Chapter 7, Preferred Alternative, identifies and discusses the Preferred Alternative. (7-7 to 7-15)
- Roadway crossings over CHSRA are Project Design Features to mitigate impacts to local transportation networks resulting from implementation of the Preferred Alternative. (3.19-7 to 3.19-9)
- 7th Road roadway is identified in Volume I: Report; Chapter 3.2, Transportation, Figure (3.2-23.)

- Impacts resulting from implementation of the Preferred Alternative are analyzed in their appropriate sections in Volume I: Report sections and in Volume II: Technical Appendices.
- 7th Road roadway designs are detailed in Volume III, Alignment Plans, Section C- Roadway and Grade Separation Plans.
- CHSRA Approved Resolution # CHSRA 14-10, attachment B approved Mitigation, Monitoring and Reporting Program for the certified FEIR/EIS with specific measures to be implemented project-wide (attached). These measures, in addition to Project Design Features, are applicable to the 7th Road underpass roadway.

Permitting

- The project has the following permits for the current design of HSR CP 2-3 (including 7th Road underpass roadway):
- Voluntary Emissions Reduction Agreement – San Joaquin Valley Air Pollution Control District
- Dust Control Plan – San Joaquin Valley Air Pollution Control District
- California Endangered Species Act Incidental Take Permit – California Department of Fish and Wildlife
- Master Streambed Alteration Agreement – California Department of Fish and Wildlife
- Biological Opinion – U.S. Fish & Wildlife Service
- Storm Water Pollution Prevention Plan – State Water Resources Control Board
- Clean Water Act Section 401 Certification – State Water Resources Control Board
- Clean Water Act Section 404 Permit- U.S. Army Corps of Engineers

**WHEREFORE, Applicant, California High-Speed Rail Authority, respectfully
requests:**

1. That the Public Utilities Commission of the State of California issue an order authorizing the construction of the 7th Road underpass grade-separated crossing under two proposed high-speed rail tracks as part of the grade-separated structure, Hanford Viaduct, on the plans and specifications set forth in Exhibit D and Exhibit E, pursuant to the provisions of Sections 1201-1205, inclusive, of the California Public Utilities Code and Commission Rules of Practice and Procedure, Rule 3.7 and Rule 3.9.
2. That the Order provides Thirty-Six (36) months from the date of such an Order within which to complete the work requested.

California High-Speed Rail Authority

A handwritten signature in black ink, appearing to read "peLnt", is enclosed within a black rectangular box. A horizontal line extends from the right side of the box.

Bruce Armistead

770 L Street, Suite 620

Sacramento, CA 95814

“List of Exhibits”

In support of the application for the proposed construction of the 7th Road underpass crossing, part of the proposed High-Speed Rail grade-separated structure, Hanford Viaduct, the following exhibits are transmitted as required by the referenced portions of CPUC Rules of Practice and Procedures, Rule 3.7 and Rule 3.9:

- EXHIBIT A: Crossing Legal Descriptions in conformance with the requirements of CPUC Rules of Practice and Procedure, Rule 3.7 (a).
 - A1 – 7th Road underpass crossing
- EXHIBIT B: Location Map; showing the location of the Project in relation to the existing roads and streets in general vicinity, in conformance with the requirements of CPUC Rules of Practice and Procedure, Rule 3.7 (e).
 - B1 – 7th Road underpass crossing
- EXHIBIT C: Vicinity Map; showing accurate locations of all streets, roads, property lines, tracks, buildings, structures or other obstructions to view in each direction from proposed crossing, in conformance with the requirements of CPUC Rules of Practice and Procedure, Rule 3.7 (d).
 - C1 – 7th Road underpass crossing
- EXHIBIT D: Plan view, Elevation, and Typical Cross Sections of the proposed roadway.
 - D1 – 7th Road underpass crossing
- EXHIBIT E: Profile Plan Drawings; showing underground lines, grade line, and rate of grades of approaches on all roadways and railroads affected by the proposed 7th Road underpass crossing, in conformance with the requirements of CPUC Rule of Practice and Procedure Rule 3.7(f).
 - E1 – 7th Road underpass crossing

- EXHIBIT F: Pavement Delineation and Sign Plan; showing pavement and signage design for roadway at 7th Road, part of the proposed SR198 Underpass Structure
 - F1 – 7th Road underpass roadway
- EXHIBIT G: Local Project Support
 - G1 – Settlement Agreement between CHSRA and Kings County
 - G2 – Concurrence Letter from City of Hanford
- EXHIBIT H: Verification for Application
- EXHIBIT I: Notice of Availability
 - The approved final Environmental Impact Report / Environmental Impact Statement (EIR/EIS) pursuant to Division 13 of the Public Resources Code, California Environmental Quality Act (CEQA). The approved document included in DVD format contains the Record of Decision (ROD), Memorandum of Agreement between the Federal Railroad Administration (FRA), CHSRA and Surface Transportation Board (STB).
- EXHIBIT J: Scoping Memo
 - J1 – 7th Road underpass crossing
- EXHIBIT K: Certification of Service

EXHIBIT A1

Crossing Legal Descriptions

7th Road underpass crossing

Dragados Flatiron Joint Venture

1775 Park Street Suite 75

Selma, CA 93662

EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0120-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land designated as Parcel 1 described in the Grant Deed recorded August 29, 2005 as Document No. 2005-28590, Official Records of said County, described as follows:

COMMENCING at the northwesterly corner of Parcel "A" as shown on that certain "PARCEL MAP filed February 26, 2007 in Book 18 of Parcel Maps, at Page 39, Records of said County; thence North 89°41'22" East 61.71 feet along the northerly line of said Parcel "A" to the **POINT OF BEGINNING**; thence North 89°41'22" East 170.00 feet along last said line; thence South 00°17'55" West 312.64 feet; thence North 90°00'00" East 8.37 feet; thence South 00°17'55" West 88.86 feet; thence North 89°42'05" West 8.37 feet; thence South 00°17'55" West 239.45 feet to the southerly line of said Parcel "A"; thence South 79°35'23" West 173.01 feet along last said line; thence North 00°17'55" East 671.25 feet to the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000066514 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.

Signature



Dan S. Scott III, PLS 7840

9/23/14

Date



EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0133-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land described in Grant Deed recorded October 17, 2000 as Document No. 2000-18694, Official Records of said County, described as follows:

COMMENCING at the northwesterly corner of Lot 1 as shown on that certain "COUNTY TRACT NO. 411, SIERRA ESTATES", filed February 5, 1981 in Volume 10 of Licensed Surveyors' Plats, page 96, Records of said County; thence South $89^{\circ}35'44''$ East 43.07 feet along the northerly line of said Lot 1 to the **POINT OF BEGINNING**; thence South $89^{\circ}35'44''$ East 152.59 feet along last said line; thence South $00^{\circ}17'55''$ West 197.94 feet to the southerly line of said Lot 1; thence South $89^{\circ}48'31''$ West 152.60 feet along last said line; thence North $00^{\circ}17'55''$ East 199.52 feet to the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000055003 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.


Signature  9/23/14
Dan S. Scott III, PLS 7840 Date



EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0135-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land Parcel described in the Grant Deed recorded April 3, 1992 as Document No. 1992-05994, Official Records of said County, described as follows:

COMMENCING at the northwesterly corner of Lot 3 as shown on that certain "COUNTY TRACT NO. 411, SIERRA ESTATES", filed February 5, 1981 in Volume 10 of Licensed Surveyors' Plats, page 96, Records of said County; thence South $89^{\circ}35'44''$ East 42.17 feet along the northerly line of said Lot 3 to the **POINT OF BEGINNING**; thence South $89^{\circ}35'44''$ East 143.84 feet along last said line to the easterly line of said Lot 3; thence South $00^{\circ}24'16''$ West 168.54 feet along last said line to the beginning of a curve concave easterly having a radius of 324.98 feet; thence southerly 77.56 feet along said curve and last said line through a central angle of $13^{\circ}40'27''$; thence South $00^{\circ}17'55''$ West 42.59 feet to the southerly line of said Lot 3; thence North $89^{\circ}35'44''$ West 152.59 feet along last said line; thence North $00^{\circ}17'55''$ East 287.96 feet to the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000055003 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.

Signature 
Dan S. Scott III, PLS 7840

9/23/14
Date



NUMBER: FB-16-0134

EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0134-1 (FEE)

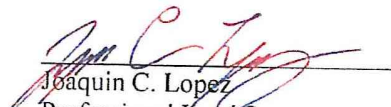
For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land described in the Individual Grant Deed recorded June 8, 1987 in Book 1413 at Page 769, Official Records of said County, described as follows:

COMMENCING at the northwesterly corner of Lot 2 as shown on that certain "COUNTY TRACT NO. 411, SIERRA ESTATES", filed February 5, 1981 in Volume 10 of Licensed Surveyors' Plats, page 96, Records of said County; thence South $89^{\circ}35'44''$ East 42.70 feet along the northerly line of said Lot 2 to the **POINT OF BEGINNING**; thence South $89^{\circ}35'44''$ East 152.59 feet along last said line; thence South $00^{\circ}17'55''$ West 198.98 feet to the southerly line of said Lot 2; thence North $89^{\circ}35'44''$ West 152.59 feet along last said line; thence North $00^{\circ}17'55''$ East 198.98 feet to the **POINT OF BEGINNING**.

Containing 30,363 square feet more or less

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 24, pages 8 to 9, Epoch 2007.00. Divide grid distances shown hereinabove by 0.999945 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.


Joaquin C. Lopez
Professional Land Surveyor
California No. 9070



8/31/15
Date

EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0136-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land described in Grant Deed recorded July 13, 2010 as Document No. 2010-11757, Official Records of said County, described as follows:

COMMENCING at the northwesterly corner of said Lot 4 as shown on that certain "COUNTY TRACT NO. 411, SIERRA ESTATES", filed February 5, 1981 in Volume 10 of Licensed Surveyors' Plats, page 96, Records of said County; thence South 89°35'44" East 41.63 feet along the northerly line of said Lot 4 to the **POINT OF BEGINNING**; thence South 89°35'44" East 144.38 feet along last said line to the easterly line of said Lot 4; thence South 00°24'16" West 292.76 feet along last said line to the southerly line of said Lot 4; thence North 89°35'44" West 143.84 feet along last said line; thence North 00°17'55" East 43.50 feet; thence North 89°42'05" West 42.09 feet to the westerly line of said Lot 4; thence North 00°24'16" East 130.00 feet along last said line; thence South 89°42'05" East 41.85 feet; thence North 00°17'55" East 119.26 feet to the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000055003 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.

Signature



Dan S. Scott III, PLS 7840

7/23/14

Date



EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0137-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land described in the Individual Grant Deed recorded April 29, 1988 in Book 1449 at Page 93, Official Records of said County, described as follows:

COMMENCING at the northwesterly corner of Lot 5 as shown on that certain "COUNTY TRACT NO. 411, SIERRA ESTATES", filed February 5, 1981 in Volume 10 of Licensed Surveyors' Plats, page 96, Records of said County; thence South $89^{\circ}35'44''$ East 41.09 feet along the northerly line of said Lot 5 to the **POINT OF BEGINNING**; thence South $89^{\circ}35'44''$ East 144.92 feet along last said line to the easterly line of said Lot 5; thence South $00^{\circ}24'16''$ West 292.76 feet along last said line to the southerly line of said Lot 5; thence North $89^{\circ}35'44''$ West 144.38 feet along last said line; thence North $00^{\circ}17'55''$ East 292.76 feet to the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000055003 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.

Signature



Dan S. Scott III, PLS 7840

7/23/14

Date



EXHIBIT "A"
LEGAL DESCRIPTION


PARCEL FB-16-0138-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land described in Grant Deed recorded August 16, 1995 as Document No. 95-14643, Official Records of said County, described as follows:

BEGINNING at the northwesterly corner of Lot 6 as shown on that certain "COUNTY TRACT NO. 411, SIERRA ESTATES", filed February 5, 1981 in Volume 10 of Licensed Surveyors' Plats, page 96, Records of said County; thence South 89°35'44" East 177.18 feet along the northerly line of said Lot 6 to the easterly line of said Lot 6, said point being the beginning of a non-tangent curve concave westerly and having a radius of 50.00 feet (a radial line from the radius point to the beginning of said curve bears North 55°49'43" East); thence southerly 30.17 feet along said curve and last said line through a central angle of 34°34'34"; thence South 00°24'16" West 265.75 feet along last said line to the southerly line of said Lot 6; thence North 89°35'44" West 144.92 feet along last said line; thence North 00°17'55" East 209.59 feet; thence North 44°42'05" West 28.28 feet; thence North 89°42'05" West 20.67 feet to the westerly line of said Lot 6; thence North 00°24'16" East 64.60 feet along last said line to the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000055003 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.

Signature 
Dan S. Scott III, PLS 7840

9/23/14
Date



EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0139-1 (FEE)

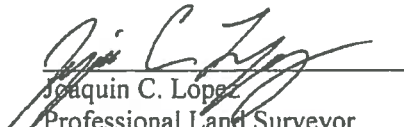
For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land described in the Individual Grant Deed recorded September 20, 2006 as Document No. 2006-27580, Official Records of said County, described as follows:

COMMENCING at the northwesterly corner of Lot 7, as shown on that certain "COUNTY TRACT NO. 411, SIERRA ESTATES", filed February 5, 1981 in Volume 10 of Licensed Surveyor's Plats, Page 96, Records of said County; thence North $79^{\circ}35'23''$ East 80.15 feet along the northerly line of said Lot 7 to the **POINT OF BEGINNING**; thence North $79^{\circ}35'23''$ East 117.04 feet along last said line; thence South $00^{\circ}20'25''$ West 159.72 feet to the northwesterly line of Ponderosa Street as shown on said map, said point also being the beginning of a non-tangent curve, concave easterly and having a radius of 50.00 feet (a radial line from the radius point to the beginning of said curve bears North $19^{\circ}38'45''$ West); thence southerly 97.17 feet along said curve and last said line, through a central angle of $111^{\circ}21'34''$ to the beginning of a reverse curve, concave southwesterly and having a radius of 50.00 feet; thence southeasterly 5.96 feet along said curve and last said line through a central angle of $06^{\circ}50'01''$ to the southerly line of said Lot 7; thence North $89^{\circ}35'44''$ West 177.18 feet along last said line to the westerly line of said Lot 7; thence North $00^{\circ}24'16''$ East 98.40 feet along last said line; thence South $89^{\circ}42'05''$ East 78.96 feet; thence North $00^{\circ}17'55''$ East 123.95 feet to the **POINT OF BEGINNING**.

Containing 32,441 square feet more or less

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 24, pages 8 to 9, Epoch 2007.00. Divide grid distances shown hereinabove by 0.999945 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.


Joaquin C. Lopez
Professional Land Surveyor
California No. 9070



8/31/15
Date

EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0144-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain parcel of land designated as Parcel 1 described in the Grant Deed recorded January 20, 2006 as Document No. 2006-02166, Official Records of said County, described as follows:

COMMENCING at the North 1/4 corner of Section 28, Township 18 South, Range 22 East, Mount Diablo Base, according to the official plat filed in the District Land Office; thence North 89°39'37" East 262.93 feet along the North line of said Section 28; thence South 00°17'55" West 30.00 feet to the **POINT OF BEGINNING**; thence South 00°17'55" West 1,293.30 feet to the southerly line of said Parcel 1; thence South 89°40'26" West 166.61 feet along last said line; thence North 00°17'55" East 462.76 feet; thence North 89°42'05" West 97.91 feet to the North-South centerline of said Section 28; thence North 00°24'16" East 158.00 feet along last said line; thence South 89°42'05" East 27.12 feet; thence North 45°17'55" East 28.28 feet; thence North 00°17'55" East 651.93 feet to a line parallel with and 30.00 feet southerly of said North line of Section 28; thence North 89°39'37" East 217.11 feet along said Parallel line the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000055003 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.


Signature  9/30/14
Dan S. Scott III, PLS 7840 Date



EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0145-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of that certain Parcel 1 described in Document No. 2006-01653, recorded January 17, 2006, Official Records of said County, said land also being a portion of the south half of the northeast quarter of Section 28, Township 18 South, Range 22 East, Mount Diablo Base, according to the official plat filed in the District Land Office, said portion described as follows:

COMMENCING at the center quarter corner of said Section 28; thence along the east-west centerline of said Section 28 North $89^{\circ}41'22''$ East 61.71 feet to the **POINT OF BEGINNING**; thence North $00^{\circ}17'55''$ East 225.08 feet; thence North $89^{\circ}42'05''$ West 61.29 feet to the north-south centerline of said Section 28; thence along said north-south centerline North $00^{\circ}24'16''$ East 125.00 feet; thence South $89^{\circ}42'05''$ East 61.06 feet; thence North $00^{\circ}17'55''$ East 411.50 feet; thence South $89^{\circ}42'05''$ East 39.50 feet; thence North $00^{\circ}17'55''$ East 561.99 feet to the north line of said south half of the northeast quarter of Section 28; thence along said north line North $89^{\circ}40'26''$ East 166.61 feet; thence South $00^{\circ}17'55''$ West 97.18 feet; thence South $44^{\circ}46'22''$ East 28.32 feet; thence South $89^{\circ}50'39''$ East 27.79 feet; thence South $00^{\circ}09'21''$ West 36.00 feet; thence North $89^{\circ}50'39''$ West 15.91 feet; thence South $00^{\circ}17'55''$ West 127.15 feet; thence North $89^{\circ}42'05''$ West 83.62 feet; thence South $00^{\circ}17'55''$ West 285.51 feet; thence South $89^{\circ}42'05''$ East 15.50 feet; thence South $00^{\circ}17'55''$ West 757.77 feet to said east-west centerline of Section 28; thence along said east-west centerline South $89^{\circ}41'22''$ West 170.00 feet to the **POINT OF BEGINNING**.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 58, pages 71 to 72, Epoch 2007.00. Multiply distances shown above by 1.000066514 to obtain ground level distances.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.

Signature 
Dan S. Scott III, PLS 7840

9/23/14
Date



EXHIBIT "A"
LEGAL DESCRIPTION

PARCEL FB-16-0151-1 (FEE)

For rail purposes, that portion of land situated in the County of Kings, State of California, being a portion of the West Half of the Northeast Quarter of Section 33, Township 18 South, Range 22 East, Mount Diablo Base and Meridian, as described in Document No. 0011052, recorded June 23, 2000, Official Records of said County, portion described as follows:

COMMENCING at the Southwest corner of said West Half of the Northeast Quarter of Section 33;

thence along the South line of said Northeast Quarter of Section 33, South 89°49'36" East 131.44 feet to the **POINT OF BEGINNING**.

thence from said **POINT OF BEGINNING**, North 00°17'55" East 2211.48 feet;

thence North 50°47'18" West 78.08 feet;

thence North 00°17'55" East 160.30 feet to the south line of 7th Road, and the beginning of a non-tangent 4,878.79 foot radius curve, the radius point of which bears South 05°13'22" East;

thence, along said south line, in a easterly direction along said non-tangent curve, through a central angle of 02°31'39" and an arc length of 215.23 feet;

thence leaving said south line, South 01°11'15" West 130.91 feet;

thence North 89°42'04" West 41.08 feet;

thence South 00°17'55" West 45.38 feet;

thence South 51°23'09" West 78.08 feet;

thence South 00°17'55" West 2211.37 feet to the south line of said Northeast Quarter of Section 33;

thence along said south line, North 89°49'36" West 50.00 feet to the **POINT OF BEGINNING**.

The above described area containing 150,200.47 square feet or 3.448 acres.

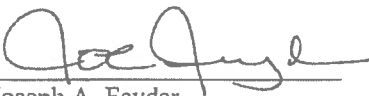
This conveyance is made for the purposes of a rail and the Owner hereby releases and relinquishes to the State any and all abutter's rights including access rights appurtenant to Owner's remaining property in and to said rail.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, as shown on Record of Survey, Book 24, Page 8, Epoch 2007.00. Multiply grid distances by 1.00004405 to obtain ground distances.

END DESCRIPTION

NUMBER: FB-16-0151-1

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act. .



Joseph A. Feyder
Professional Land Surveyor
California No. 5901



12-22-2014

Date

EXHIBIT "A"
LEGAL DESCRIPTION

FB-16-0420-1 (Fee)

That portion of the West-half of the Northeast-quarter, of Section 33, Township 18 South, Range 22 East, Mount Diablo Meridian, Kings County, California, as described in a Quitclaim Deed recorded as 0011052, on June 23, 2000, Official Records of said Kings County, described as follows:

COMMENCING at the Center-quarter corner of said Section 33, marked with a 1 inch iron pipe, with plug stamped "R.C.E. 21489", as shown on a map filed in Book 2 of Parcel Maps at Page 84, records of said County;

thence along the South line of said Northeast-quarter of Section 33, South 89°49'03 East a distance of 81.44 feet, to the true **POINT OF BEGINNING**;

thence continuing along said South line, South 89°49'03 East a distance of 50.00 feet, to the southwest corner of Parcel FB-16-0151-1, as described in a STIPULATION FOR ORDER FOR POSSESSION AND ORDER FOR POSSESSION filed by the Clerk of Court, Superior Court of the State of California, County of Kings, noted as Case No. 15-C0306 dated November 10, 2016;

thence along the easterly line of said Parcel FB-16-0151-1 the following two courses:

Course 1) North 00°17'55" East a distance of 2,211.29 feet;

Course 2) North 50°47'18" West a distance of 64.26 feet, to a line parallel with and 50-feet west from said Course 1;

thence along said parallel line, South 00°17'55" West a distance of 2,251.76 feet, to the **POINT OF BEGINNING**.

The above described parcel contains 111,576 square feet, more or less.

This conveyance is made for the purposes of a rail and the Grantors hereby release and relinquish to the California High-Speed Rail Authority and the State any and all abutter's rights, including access rights appurtenant to Owner's remaining property in and to said rail.

The grantors further understand that the present intention of the grantee is to construct and maintain a public rail on the lands hereby conveyed in fee and the grantors, for the grantors and the grantors' successors and assigns, hereby waive any claims for any and all damages to grantors' remaining property contiguous to the property hereby conveyed by reason of the location, construction, landscaping or maintenance of said rail.

Bearings and distances in the above description are based on the California Coordinate System 1983, Zone 4, Epoch 2007.00, as shown on Record of Survey recorded in Volume 24 of Licensed Surveyors' Plats at Page 8, Kings County Records. Divide grid distances herein above by 0.999955952 to obtain ground level distances.

END OF DESCRIPTION

This real property description has been prepared by me, or under my Direction, in conformance with the Professional Land Surveyors' Act.

Signature:

Richard R. Hernandez

Date: 8/1/2017



FINAL

EXHIBIT "A"

LEGAL DESCRIPTION

PARCEL FB-16-0448-3

That portion of Lots 1, 2, and 3, County Tract No. 411, Sierra Estates, filed in Volume 10, Page 96 of Licensed Surveyor Records on February 5, 1981, Official Records of Kings County, lying within the Southeast Quarter of the Section 28, Township 18 South, Range 22 East, Mount Diablo Meridian, County of Kings, State of California, said portion described as follows:

COMMENCING at the Southwest Corner of said Section 28, as described on Kings County Corner Record No. 2736 filed in the Kings County Surveyor's Records; thence North 89°48'59" East along the South line of said Southwest Quarter a distance of 2644.52 feet to the East line of said Southwest Quarter; thence North 00°24'16" East along said East line a distance of 40.00 feet to the Southwest Corner of said Lot 1; thence North 89°48'31" East along the South line of said Lot 1 a distance of 196.04 feet to the **POINT OF BEGINNING**; thence North 00°17'55" East a distance of 439.51 feet to a East line of said Lot 3 and the beginning of a non-tangent curve to the left, to which a radial line from the radius point to the beginning of curve bears South 76°43'50" West; thence along the East lines of said Lots 2 and 3 and the arc of said curve having a radius of 324.98 feet, a central angle of 24°35'24", and an arc distance of 139.47 feet; thence South 00°17'55" West a distance of 117.14 feet; thence North 89°35'44" West a distance of 19.88 feet; thence South 00°17'55" West a distance of 65.39 feet; thence South 89°42'05" East a distance of 100.00 feet; thence South 00°17'55" West a distance of 70.52 feet to the beginning of a non-tangent curve to the left, to which a radial line from the radius point to the beginning of curve bears North 07°10'09" West; thence along the arc of said curve having a radius of 62.00 feet, a central angle of 82°17'36", and an arc distance of 89.05 feet to said South line; thence South 89°48'31" West along said South line a distance of 86.56 feet to the **POINT OF BEGINNING**.

The above described parcel contains 28,823 sq. ft. (0.662 acres), more or less.

EXCEPTING THEREFROM unto the Union Central Life Insurance Company, its successors and assigns an undivided one-half of the oil, gas and other hydrocarbons and minerals now or at any time hereafter situate therein and thereunder, together with all easements and rights necessary or convenient for the production, storage and transportation thereof, as reserved in Deed recorded November 9, 1944 in Thomas P. Dutra, in Book 316 Page 105 Official Records.

ALSO EXCEPTING THEREFROM unto Thomas P. Dutra, 1/4 of all oil, gas and other hydrocarbon substances and minerals, as reserved in Deed to Ivan R. Wall, recorded April 27, 1950 in Book 449, Page 475, Official Records.

ALSO EXCEPTING THEREFROM unto Evelyn Otis, formerly Evelyn Wall, dealing with her sole and separate property, an undivided one-eighth interest in and to all oil, gas and other hydrocarbon substances, in, on or under said land as reserved in Deed to Wilford A. Maxwell and Kathleen C. Maxwell, recorded February 15, 1956 in Book 639 Page 486 Official Records.

IN ADDITION TO THE UNDIVIDED 7/8 INTEREST RESERVED ABOVE, the Grantor, her heirs and assigns, herein reserve the remaining undivided 1/8 interest in all oil, gas and other hydrocarbon substances, now or at any time hereafter situate therein and thereunder, below a

depth of 500 feet, as reserved in Deed dated February 29, 1980, executed by Kathleen C. Maxwell, a widow, to Embree-Bogard Company, a California corporation, and recorded February 10, 1981 in Book 1193 at Page 290 Official Records, as Document No.1576.

Lands abutting said portion shall have no right or easement of access thereto.

The bearings and distances used in the above description are based on the California Coordinate System 1983, Zone 4, Epoch 2007.00, as shown on Record of Survey recorded in Volume 24 of Licensed Surveyors' Plats, Page 8, Kings County Records. Divide distance shown above by 0.99994500 to obtain ground distance.

This real property description has been prepared by me, or under my direction, in conformance with the Professional Land Surveyors' Act.



Gordon N. Anderson, LS 9239



18 November 2016
Date

EXHIBIT "A"
LEGAL DESCRIPTION

FB-16-0468-1 (Fee)

That portion of the West-half of the Northeast-quarter, of Section 33, Township 18 South, Range 22 East, Mount Diablo Meridian, as described in a Quitclaim Deed recorded as Document No. 0011052, on June 23, 2000, Official Records of the County of Kings, State of California, described as follows:

COMMENCING at the Center-quarter corner of said Section 33, marked with a 1-inch iron pipe, with a plug stamped "R.C.E. 21489", as shown on a map filed in Book 2 of Parcel Maps at Page 84, records of said County;

thence along the south line of said Northeast-quarter of Section 33, South 89°49'03" East a distance of 181.44 feet, to the true **POINT OF BEGINNING**;

thence continuing along said south line, South 89°49'03" East a distance of 50.00 feet;

thence along a line hereinafter called Line "A", North 00°17'55" East a distance of 2,251.45 feet;

thence South 51°23'09" West a distance of 64.26 feet, to a line parallel with and 50-feet west of said Line "A";

thence along said parallel line, South 00°17'55" West a distance of 2,211.19 feet, to the **POINT OF BEGINNING**.

The above described parcel contains 111,566 square feet, more or less.

This conveyance is made for the purposes of a rail and the Grantors hereby release and relinquish to the California High-Speed Rail Authority and the State any and all abutter's rights, including access rights appurtenant to Owner's remaining property in and to said rail.

The grantors further understand that the present intention of the grantee is to construct and maintain a public rail on the lands hereby conveyed in fee and the grantors, for the grantors and the grantors' successors and assigns, hereby waive any claims for any and all damages to grantors' remaining property contiguous to the property hereby conveyed by reason of the location, construction, landscaping or maintenance of said rail.

Bearings and distances in the above description are based on the California Coordinate System 1983, Zone 4, Epoch 2007.00, as shown on Record of Survey recorded in Volume 24 of Licensed Surveyors' Plats at Page 8, Kings County Records. Divide grid distances herein above by 0.999955952 to obtain ground level distances.

END OF DESCRIPTION

This real property description has been prepared by me, or under my Direction, in conformance with the Professional Land Surveyors' Act.

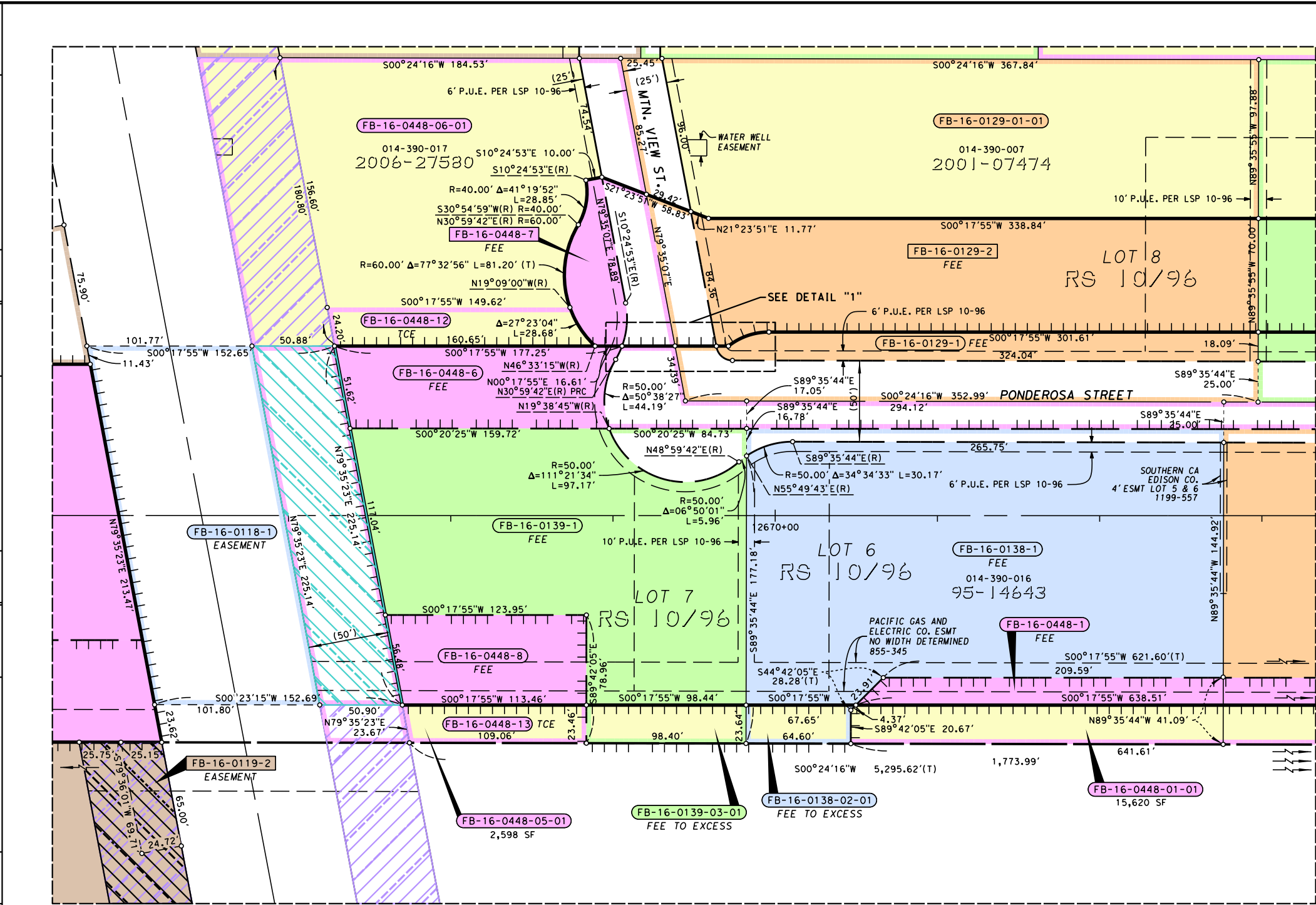
Signature:

Richard R. Hernandez

Date: 7/27/2017



DATE	BY	REVISIONS	DATE	BY	REVISIONS	DATE	BY	REVISIONS	DATE	BY	REVISIONS
05-30-17	REVISIONS	03-23-18	REMOVED 0441-1, 4, 7; 0448-9	R. HERNANDEZ							
08-01-17	REVISIONS	03-23-18	REVISED 0448-1, 01-01	R. HERNANDEZ							
01-04-18	REVISIONS	09-20-18	UPRR RW FROM 200' TO 100'	D. CRAWFORD							
03-23-18	REVISIONS	10-21-19	ADD P.U.E. PARCELS -0139,	E. BACKLIN							
03-23-18	REVISIONS	01-31	ADDED -02-01 FROM -1								
03-23-18	REVISIONS	01-31	ADDED -03-01 FROM -1								

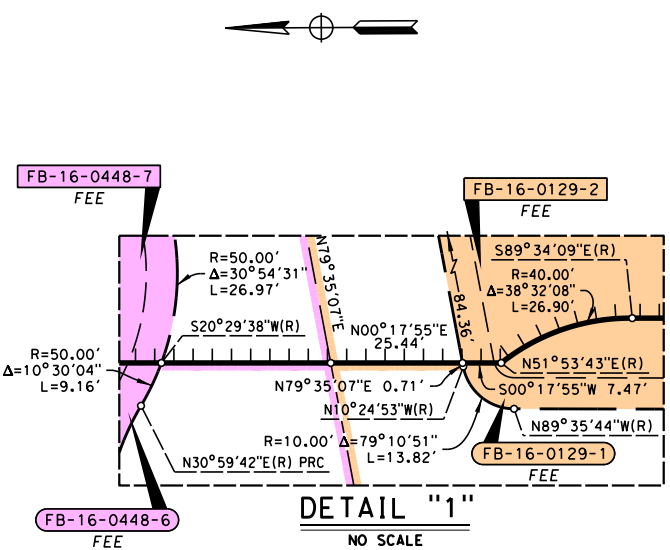


NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

GORDON N. ANDERSON
PROFESSIONAL LAND SURVEYOR
10-14-2016
DATE

PROFESSIONAL LAND SURVEYOR
GORDON N. ANDERSON
LS9239
STATE OF CALIFORNIA

COUNTY OF KINGS
T.18S. R.22E. M. D. M.



NOTE

- POSSIBLE FUTURE PERMANENT NEED
- POSSIBLE FUTURE TEMPORARY NEED
- CLOUDED TITLE

PARCEL#	TITLE CODE	GRANTOR	AREAS ① (square feet or as noted)				REMARKS
			TOTAL	REQUIRED ① [UF]	EXCESS ① [UF]	REMAINDER	
FB-16-0118-1	E	Southern Pacific Transportation Co		22,497			FOR RAIL PURPOSES
FB-16-0119-2	E	EBC Farms, LLC					SEE MAP RW-M4027-2K1
FB-16-0129-1	F	Beverly Jean Long	65,091	5,786 [9,121]	35,180 [2,266]		LOT 8 P.U.E. 3,745 SF
FB-16-0129-2	F	Beverly Jean Long		24,125 [2,244]			COUNTY R/W-PONDEROSA ST
FB-16-0138-1	F	Ramon B Jr & Mary E Castillo	54,629	45,445 [7,433]	10,719		1,535 SF CONVERTED TO EXCESS FB-16-0138-02-01. LOT 6 P.U.E. 2,385 SF.
FB-16-0139-1	F	Darryl Archer	82,655	32,441 [2,170]	52,532 [7,503]		2,318 SF CONVERTED TO EXCESS FB-16-0139-03-01. LOT 7 P.U.E. 2,185 SF.
FB-16-0448-1	F	State of California		10,652			
FB-16-0448-6	F	State of California		8,352 [12,740]			
FB-16-0448-7	F	State of California		2,642 [2,577]			
FB-16-0448-8	F	State of California		6,588			
FB-16-0448-12	TCE	State of California		3,656			
FB-16-0448-13	TCE	State of California		2,598			COUNTY R/W-PONDEROSA ST & MTN VIEW ST

GRANTOR NOTES	NOTES
① Areas shown exclude underlying fee in the adjoining public way. Ac=acres, SF=Square Feet	Horizontal Datum: NAD83 (NRS2007), California Coordinate System of 1983. Coordinates and bearing are CCS83, Zone 4, Epoch 2007.00. Distances and stationing are grid distances. Divide by 0.999945000 to obtain ground level distances. All distances are in feet (U.S. Survey Feet).
② TITLE CODES: F=Fee E=Easement (Ease) TCE=Temp Construction Ease F2X=Fee converted to Excess X2F=Excess converted to Fee F2E=Fee converted to Ease E2F=Ease converted to Fee O=Other (see Remarks)	LEGEND Access Prohibited Access Superseded Existing R/W Superseded Access Opening (Private) Radial Bearing Total Distance Section Corner Found Monument as noted Calculated point (Not monument symbol) Title to State Required for Others HST R/W High-Speed Train R/W

CALIFORNIA HIGH-SPEED TRAIN PROJECT
FRESNO TO BAKERSFIELD

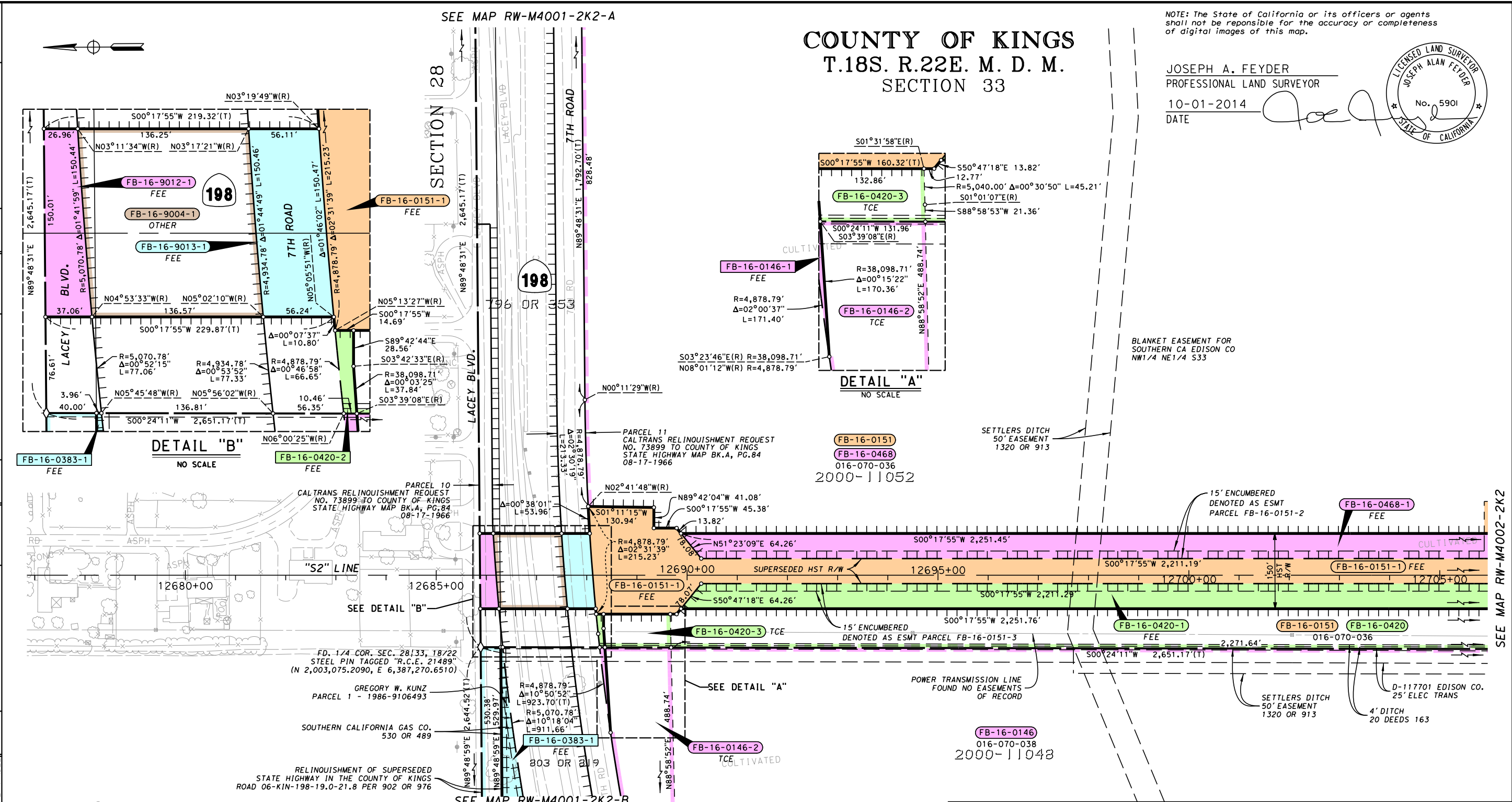
**RIGHT OF WAY
APPRAISAL MAP
RW-M4027-2K1-E**

NO SCALE

CALIFORNIA
HIGH-SPEED RAIL AUTHORITY

CONTRACT	COUNTY	PM	DWG No.	SHEET NO.	TOTAL SHEETS
CP2-3	KIN		RW-M4027-2K1-E	51	60

R/W PROJECT SURVEYOR: J. FEYDER	REVISIONS		BY	REVISIONS		DATE	REVISIONS		BY	REVISIONS		DATE	REVISIONS		BY
	DATE	DESCRIPTION		DATE	DESCRIPTION		DATE	DESCRIPTION		DATE	DESCRIPTION		DATE	DESCRIPTION	
11-30-16	ADDED FB-16-0146-1,2		G. ANDERSON	05-08-17	REVISED PARCEL DIMENSIONS & CORRECTED DIMENSIONS				R. HERNANDEZ						
11-30-16	ADDED FB-16-0383-1		G. ANDERSON	10-10-17	UF ADDED FOR 0383-1				R. HERNANDEZ						
11-30-16	ADDED FB-16-0420-1,2,3,4		G. ANDERSON	03-22-18	ADDED 9004-1, 9012-1, 9013-1				R. HERNANDEZ						
11-30-16	ADDED FB-16-9004-1,2,3		G. ANDERSON	04-01-19	0420, REVISED DIMENSION				R. HERNANDEZ						
05-08-17	REMOVED INCORRECT FB-16-9004		R. HERNANDEZ												



PARCEL#	TITLE CODE	GRANTOR	AREAS (square feet or as noted)				REMARKS
			TOTAL	REQUIRED (UF)	EXCESS (UF)	REMAINDER	
FB-16-0146-1	F	Gloria J. Coelho, Trustee	68.80 Ac	985		68.78 Ac	CALTRANS R/W - SR-198; 25' Esmt 3,525 SF
FB-16-0146-2	TCE	Gloria J. Coelho, Trustee		69,846			
FB-16-0151-1	F	Gloria J. Coelho, Trustee	74.09 Ac	150,197		70.64 Ac	50' Esmt 2,500 SF
FB-16-0383-1	F	Elsie Levario, Trustee					CALTRANS R/W - SR-198; SEE MAP RW-M4001-2K2-B
FB-16-0420-1	F	Gloria J. Coelho, Trustee	301,401	111,576		189,024	78,316 SF NOT ENCUMBERED BY FB-16-0151-3
FB-16-0420-2	F	Gloria J. Coelho, Trustee		801			CALTRANS R/W - SR-198; 4' Esmt 43 SF
FB-16-0420-3	TCE	Gloria J. Coelho, Trustee		8,837			4' Esmt 529 SF
FB-16-0468-1	F	Gloria J. Coelho, Trustee	63.72 Ac	111,566		61.16 Ac	73,933 SF NOT ENCUMBERED BY FB-16-0151-2
FB-16-9004-1	O	State of California		20,460			CALTRANS-TRANSFER OF JURISDICTION
FB-16-9012-1	F	County of Kings		4,745			
FB-16-9013-1	F	County of Kings		8,426			

GRANTOR NOTES		NOTES	
①	Areas shown exclude underlying fee in the adjoining public way. Ac=acres, SF=Square Feet	Horizontal Datum: NAD83 (NRS2007), California Coordinate System of 1983. Coordinates and bearing are CC83, Zone 4, Epoch 2007.00. Distances and stationing are grid distances. Divide by 0.99995552 to obtain ground level distances. All distances are in feet (U.S. Survey Feet).	
②	Indicates Underlying Fee (UF) Area		
	Indicates Indeterminate UF		
	TITLE CODES:		
	F=Fee		
	E=Easement (Ease)		
	TCE=Temp Construction Ease		
	F2X=Fee converted to Excess		
	X2F=Excess converted to Fee		
	F2E=Fee converted to Ease		
	E2F=Ease converted to Fee		
	O=Other (see Remarks)		
LEGEND			
-----	Access Prohibited		
-----	Access Superseded		
-----	Existing R/W Superseded		
-----	Access Opening (Private)		
-----	Radial Bearing		
-----	Total Distance		
-----	Section Corner		
-----	Found Monument as noted		
-----	Calculated point		
-----	(Not monument symbol)		
-----	Title to State		
-----	Required for Others		
-----	HST R/W		

CALIFORNIA HIGH-SPEED TRAIN PROJECT
FRESNO TO BAKERSFIELD

RIGHT OF WAY
APPRAISAL MAP
RW-M4001-2K2

FEET 0 50 100 200 300

CONTRACT	COUNTY	PM	DWG No.	SHEET NO.	TOTAL SHEETS
CP2-3	KIN		RW-M4001-2K2	1	36

COUNTY OF KINGS
T.18S. R.22E. M. D. M.
SECTION 33

NOTE: The State of California or its officers or agents shall not be responsible for the accuracy or completeness of digital images of this map.

JOSEPH A. FEYDER
PROFESSIONAL LAND SURVEYOR
10-01-2014
DATE

LICENSED LAND SURVEYOR
JOSEPH ALAN FEYDER
No. 5901
STATE OF CALIFORNIA

DETAIL "A"
NO SCALE

DETAIL "B"
NO SCALE

SEE MAP RW-M4001-2K2-B

SEE MAP RW-M4001-2K2-A

BLANKET EASEMENT FOR
SOUTHERN CA EDISON CO
NW1/4 NE1/4 S33

SEE MAP RW-M4002-2K2

EXHIBIT B1

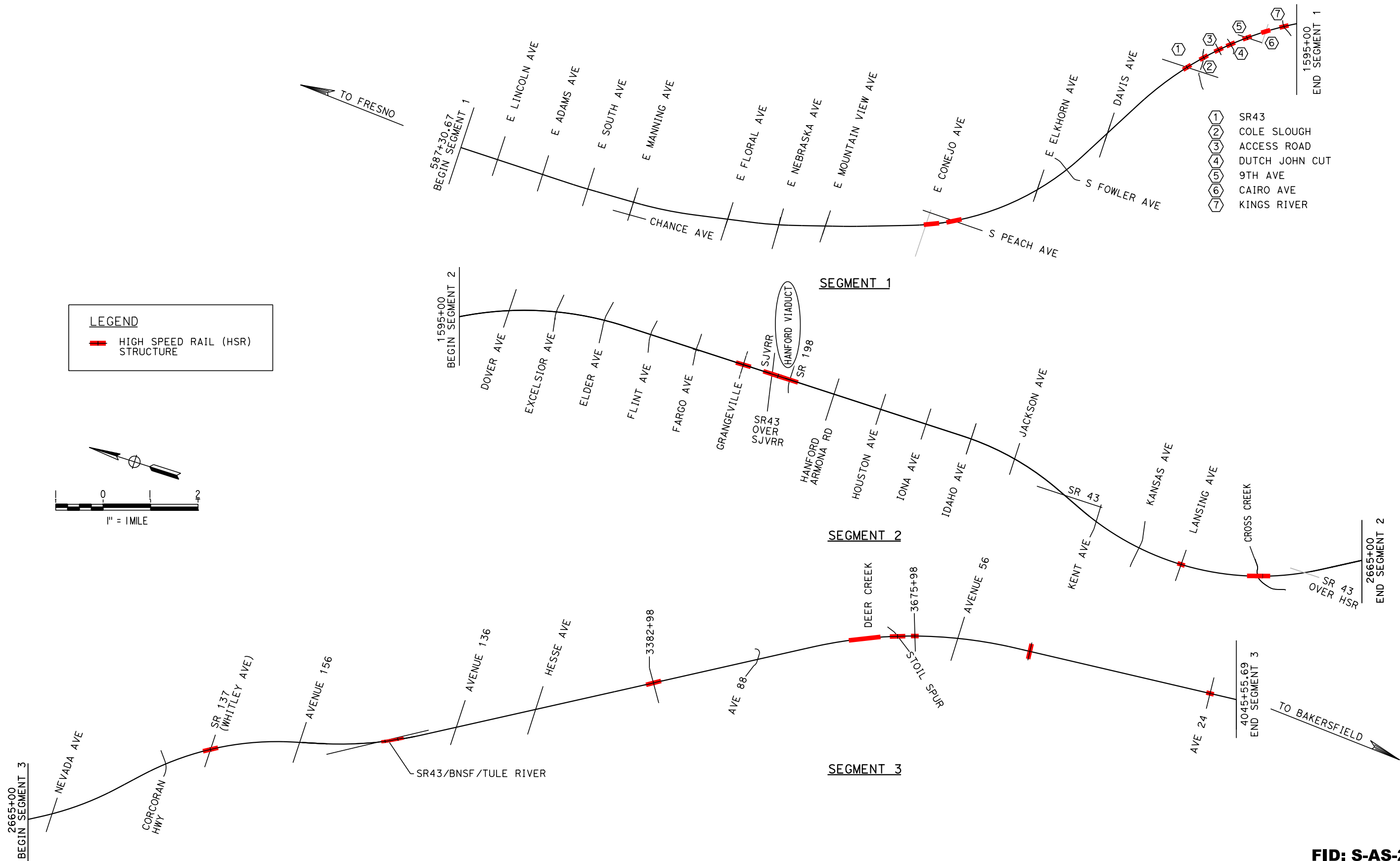
Locations Map

7th Road underpass crossing

Dragados Flatiron Joint Venture

1775 Park Street Suite 75

Selma, CA 93662



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
M. LEWIS

DRAWN BY
M. SPURGEON

CHECKED BY
S. VARELA

IN CHARGE
R. TREVETT

DATE
03/20/20



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3

SEGMENT 1, 2 & 3
HANFORD VIADUCT
STRUCTURES KEY MAP - HSR

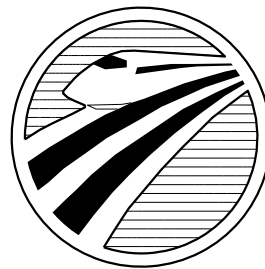
CONTRACT NO. HSR 13-57
DRAWING NO. ST-J0005-HNV
SCALE NO SCALE
SHEET NO. 3 OF 66

EXHIBIT C1

Vicinity Map

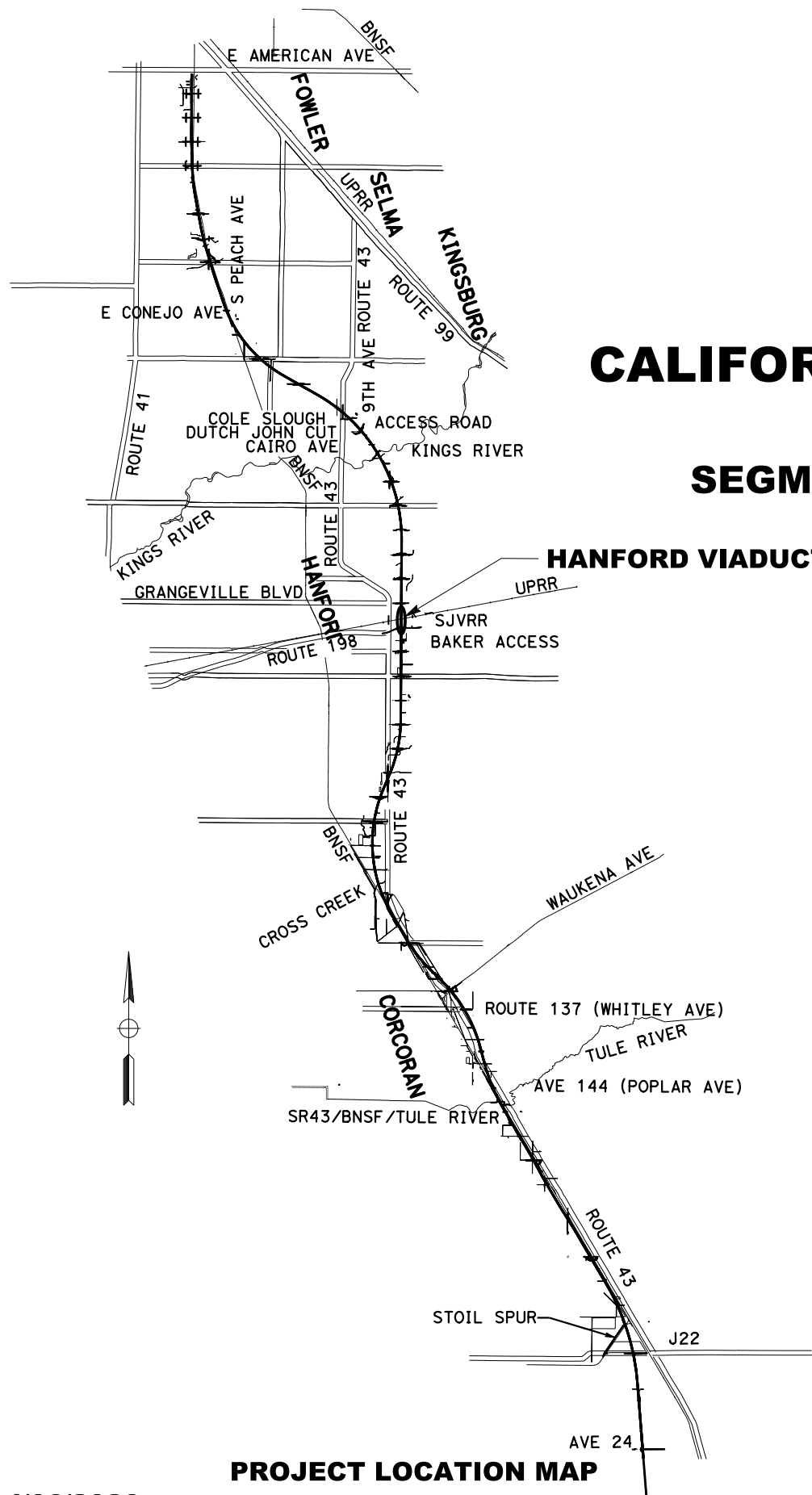
7th Road underpass crossing

**Dragados Flatiron Joint Venture
1775 Park Street Suite 75
Selma, CA 93662**



CALIFORNIA
HIGH-SPEED RAIL AUTHORITY

RFC SUBMITTAL
CALIFORNIA HIGH-SPEED TRAIN PROJECT
SIERRA SUBDIVISION
SEGMENT 2 / CONSTRUCTION PACKAGE 2-3
HANFORD VIADUCT
SUBSTRUCTURE PACKAGE



PROJECT LOCATION MAP



**CALIFORNIA
HIGH-SPEED
TRAIN PROJECT
MAP**

**PROJECT
LOCATION**



FID: S-AS-223.0

DATE: 04/06/2020

CONTRACT No. **HSR13-57**

EXHIBIT D1

Structural Drawings

7th Road underpass crossing

Dragados Flatiron Joint Venture

1775 Park Street Suite 75

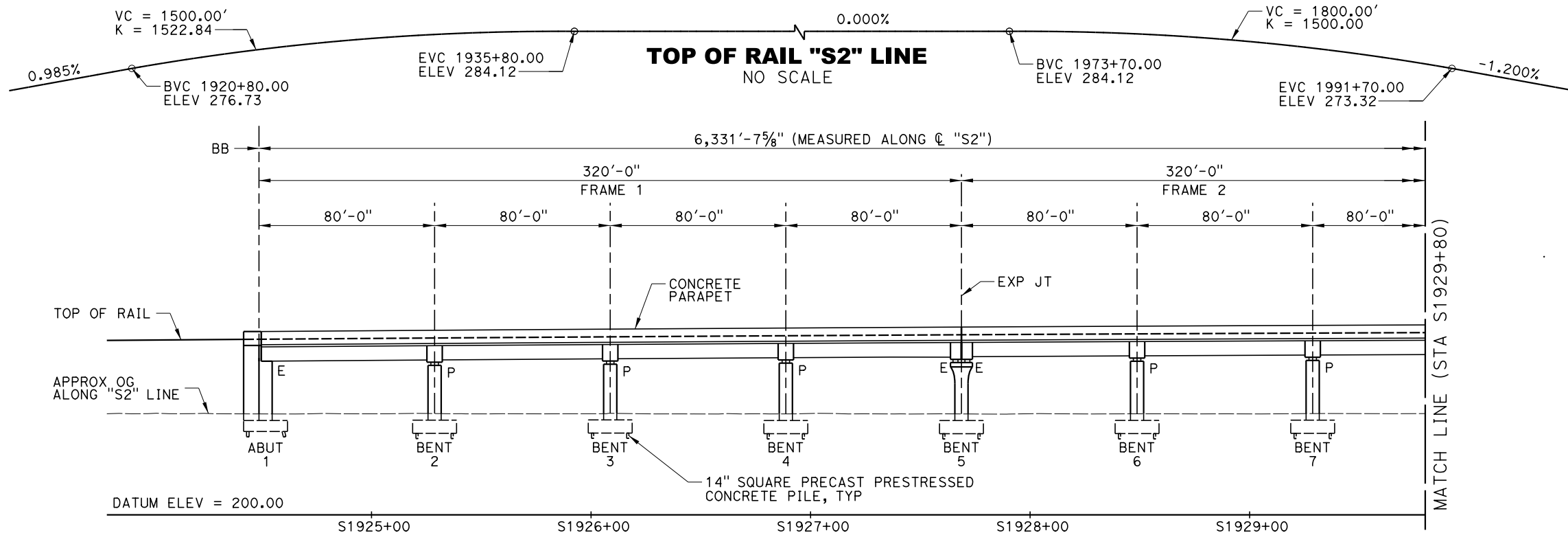
Selma, CA 93662

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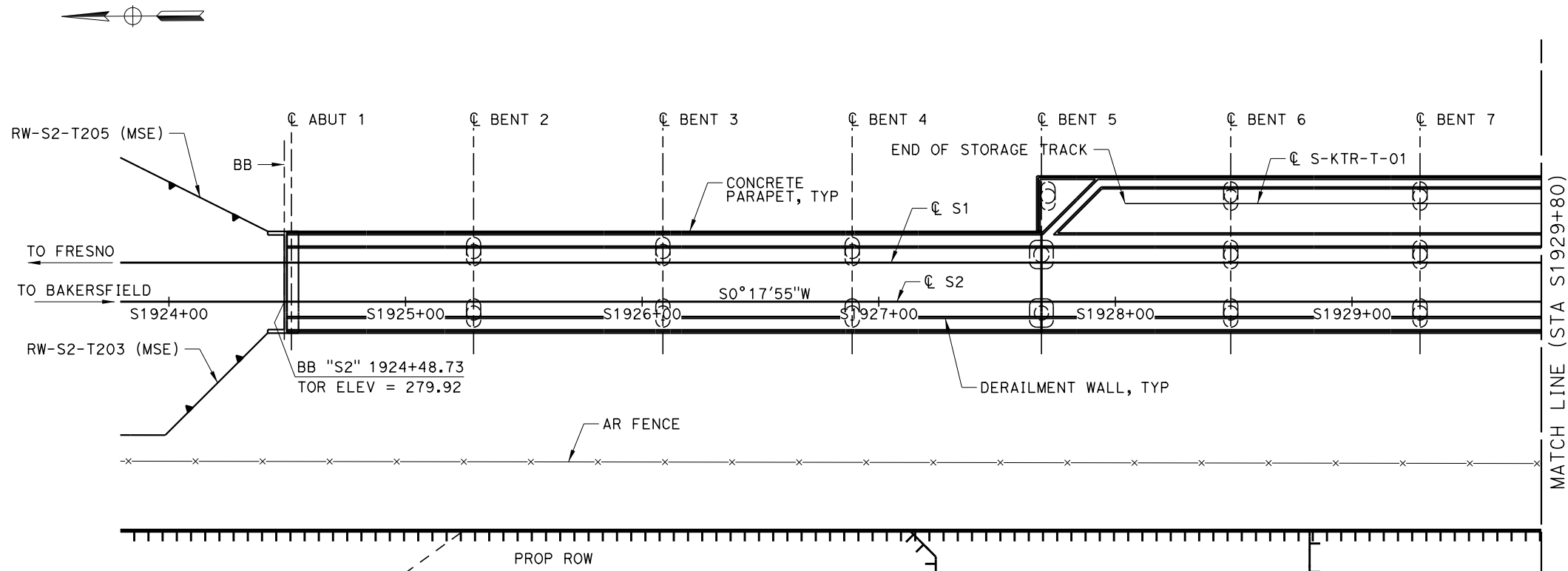
CAHSR RFC_SIGNED.tbl iplofdr-vn.plt

7/24/2020 10:11:48 AM

USL450-APP064\$



ELEVATION
SCALE: 1"=30'



PLAN
SCALE: 1"=30'

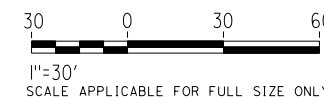
NOTES:

1. FOR TYPICAL SECTIONS, SEE DRAWING NO. ST-J1013-HNV, ST-J1014-HNV AND ST-J1015-HNV.
2. FOR UTILITY INFORMATION, SEE COMPOSITE UTILITY PLANS.
3. APPLY KINEMATIC DOT FORMLINER TO CONCRETE PARAPET PER AESTHETIC GUIDELINES.
4. FOR RETAINING WALL LAYOUT AND ELEVATION, REFER TO SEGMENT 2 TRACK GUIDEWAY PACKAGE. FOR RETAINING WALL DETAILS, REFER TO GENERAL PACKAGE PART 3 RETAINING WALLS.
5. DIMENSION IS PERPENDICULAR TO SJVR TRACK.
6. MINIMUM VERTICAL CLEARANCE BELOW STRUCTURE WITHIN THE SJVR ROW.
7. FOR OCS POLE LAYOUT, SEE DRAWING NO. ST-J5098-HNV, ST-J5099-HNV AND ST-J5100-HNV.

LEGEND:

- INDICATES POINT OF MINIMUM VERTICAL CLEARANCE
- BB BEGIN BRIDGE
- EB END BRIDGE
- E CONNECTION IS FREE TO MOVE LONGITUDINALLY
- P PINNED CONNECTION

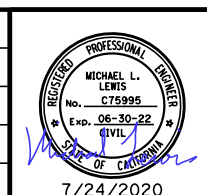
THIS SHEET IS PART OF AN EARLY RELEASE CONSTRUCTION PACKAGE, WHICH IS BEING CONSTRUCTED AT THE CONTRACTOR'S RISK. THIS SHEET SHALL BE DISREGARDED AS SOON AS THE RFC PACKAGE FOR THESE SHEETS HAVE BEEN APPROVED BY THE AUTHORITY AND ISSUED FOR CONSTRUCTION. DFJV ACCEPTS ALL RESPONSIBILITY AND LIABILITY FOR ANY CONSTRUCTION CHANGES THAT OCCUR PRIOR TO FINAL APPROVAL OF THE RELEASE FOR CONSTRUCTION PACKAGE.



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION
1	7/24/20	ML	JB	GF	DESIGN UPDATE

DESIGNED BY M. LEWIS
DRAWN BY M. SPURGEON
CHECKED BY S. VARELA
IN CHARGE G. FROMM
DATE 07/24/2020



**CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3**

SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
1 OF 15

CONTRACT NO. HSR 13-57
DRAWING NO. ST-J1003-HNV
SCALE AS SHOWN
SHEET NO. 8 OF 279

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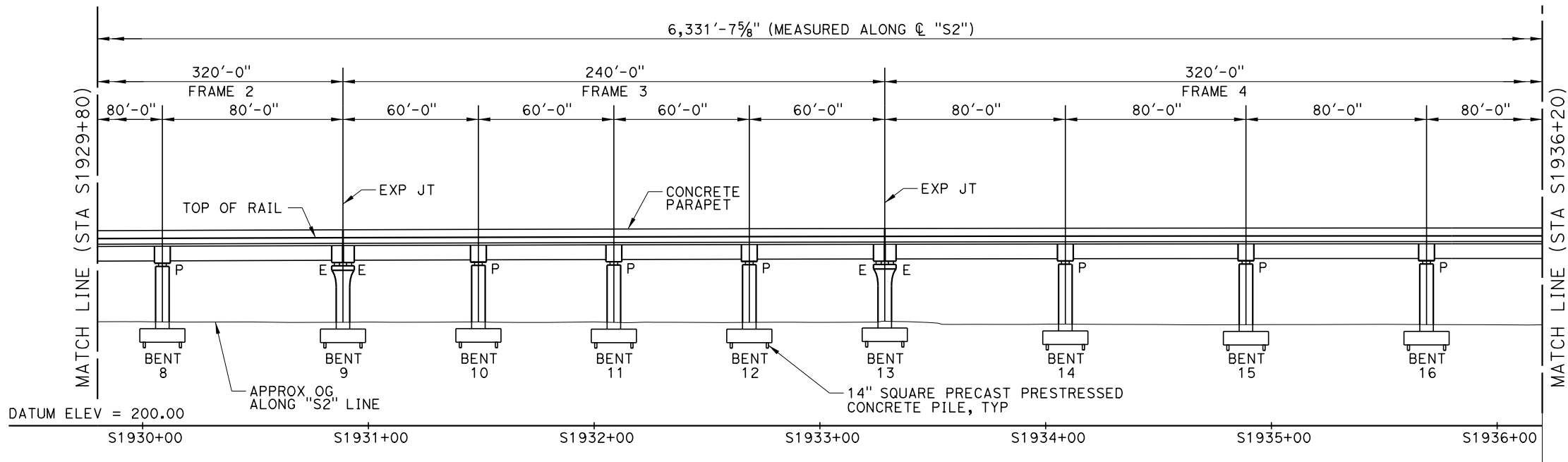
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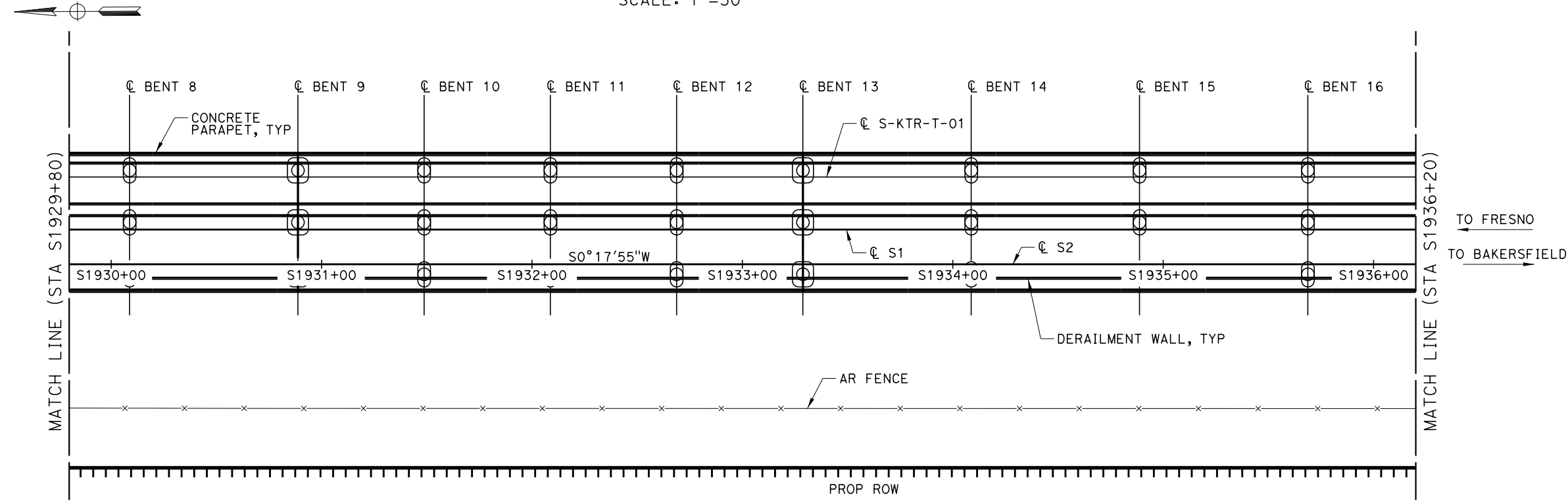
USL450-APP064\$

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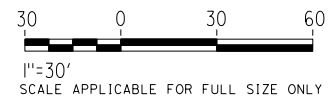
1. FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.



ELEVATION
SCALE: 1"=30'



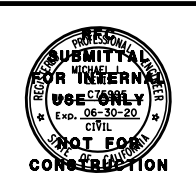
PLAN
SCALE: 1"=30'



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
R. TREVETT
DATE
04/06/20



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3
SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
2 OF 13

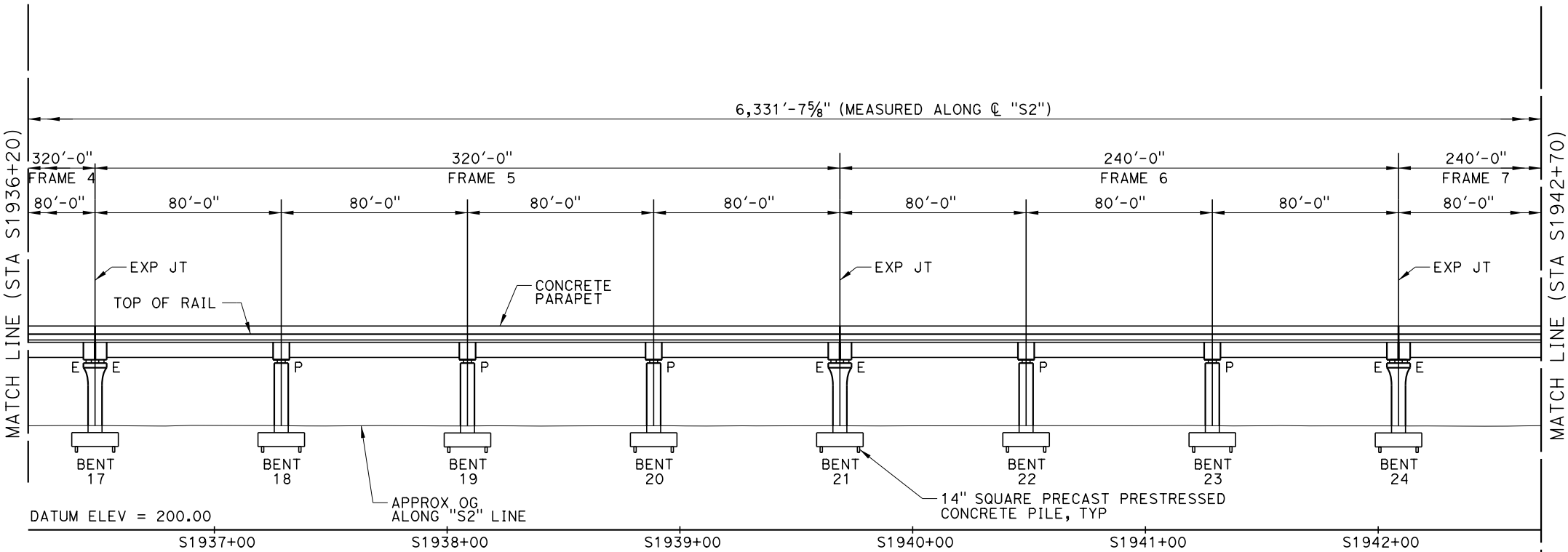
CONTRACT NO.
HSR 13-57
DRAWING NO.
ST-J1004-HNV
SCALE
AS SHOWN
SHEET NO.
7 OF 66

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4/3/2020 1:01:51 PM

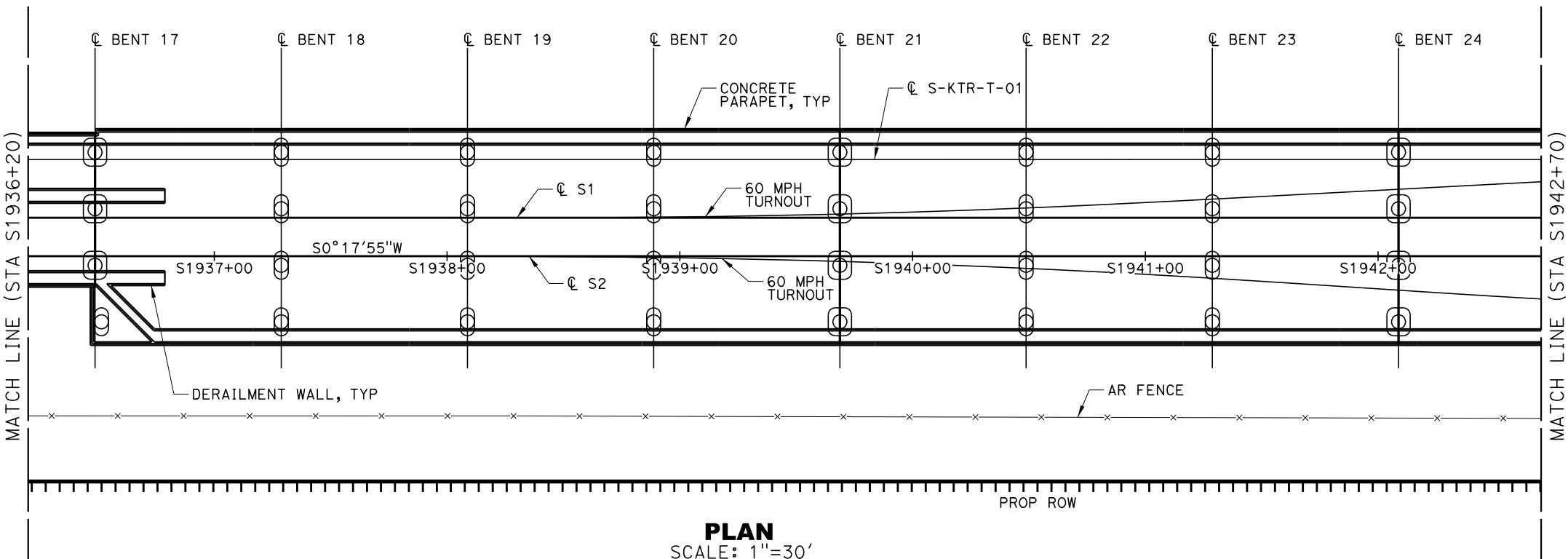
USLASO-APP064\$



ELEVATION
SCALE: 1"=30'

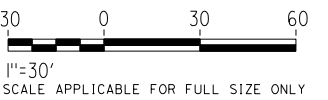
NOTES:

1. FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.



PLAN
SCALE: 1"=30'

TO FRESNO
TO BAKERSFIELD



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
R. TREVETT
DATE
04/06/20



**CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3**

SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
3 OF 13

CONTRACT NO. HSR 13-57
DRAWING NO. ST-J1005-HNV
SCALE AS SHOWN
SHEET NO. 8 OF 66

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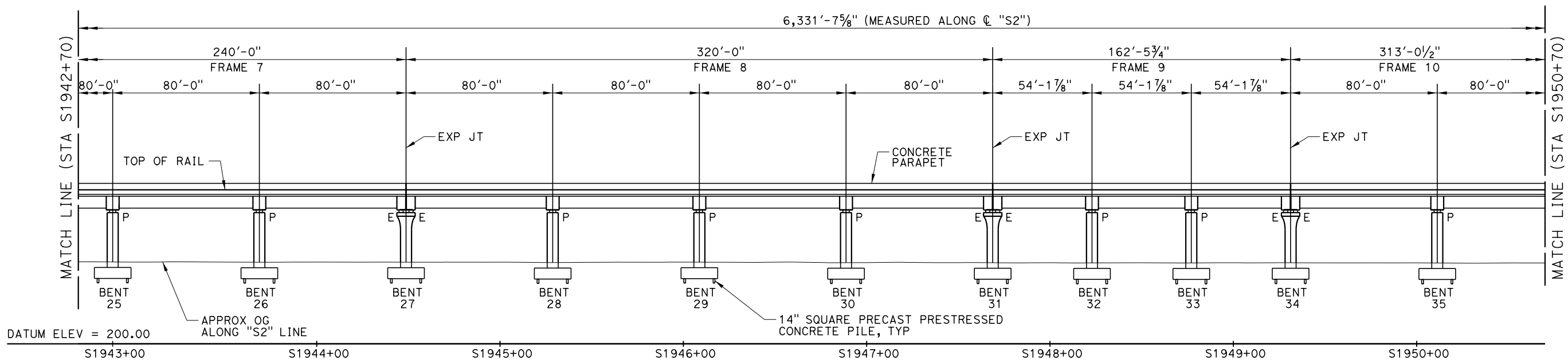
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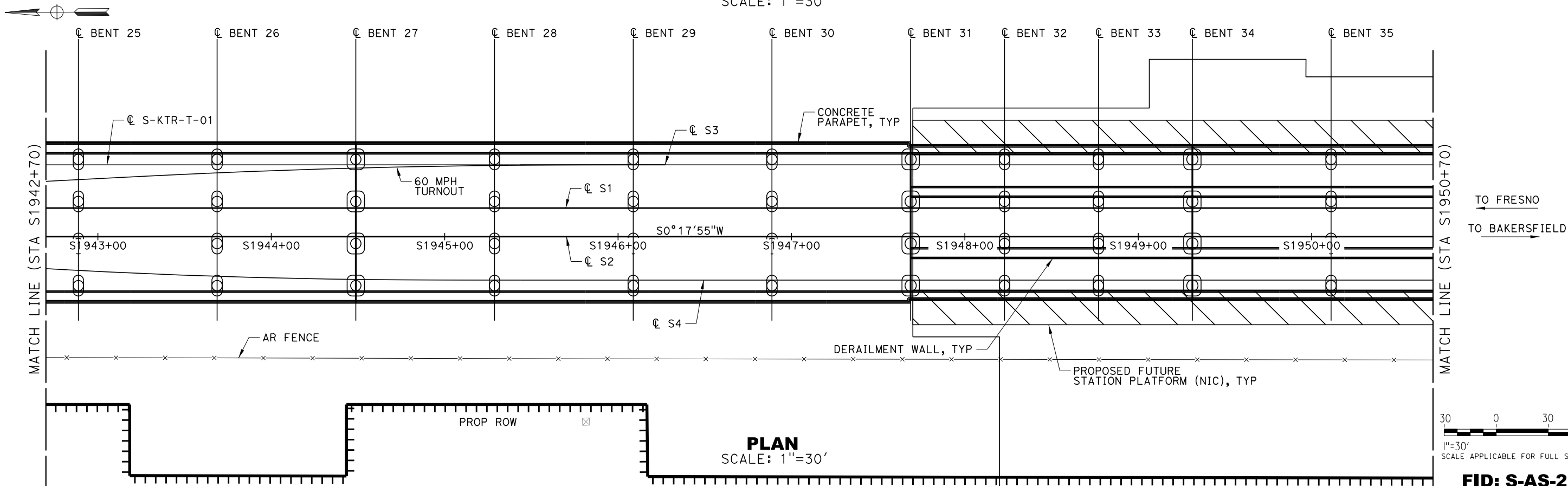
USL450-APP064\$

NOTES:

1. FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.



ELEVATION
SCALE: 1"=30'



FID: S-AS-223.0

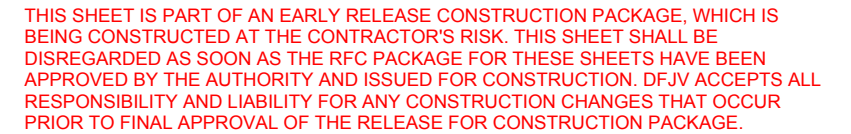
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
R. TREVETT
DATE
04/06/20



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3
SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
4 OF 13

CONTRACT NO.
HSR 13-57
DRAWING NO.
ST-J1006-HNV
SCALE
AS SHOWN
SHEET NO.
9 OF 66



CONTRACT NO.	HSR 13-57
DRAWING NO.	ST-J1007-HNV
SCALE	AS SHOWN
SHEET NO.	12 OF 279

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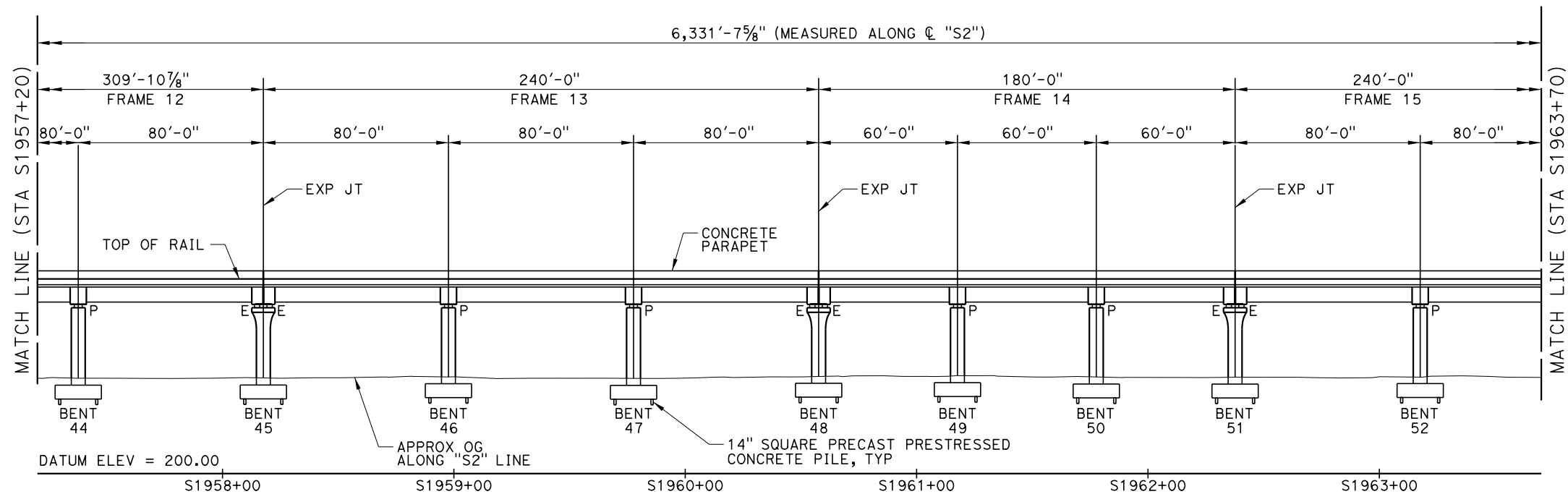
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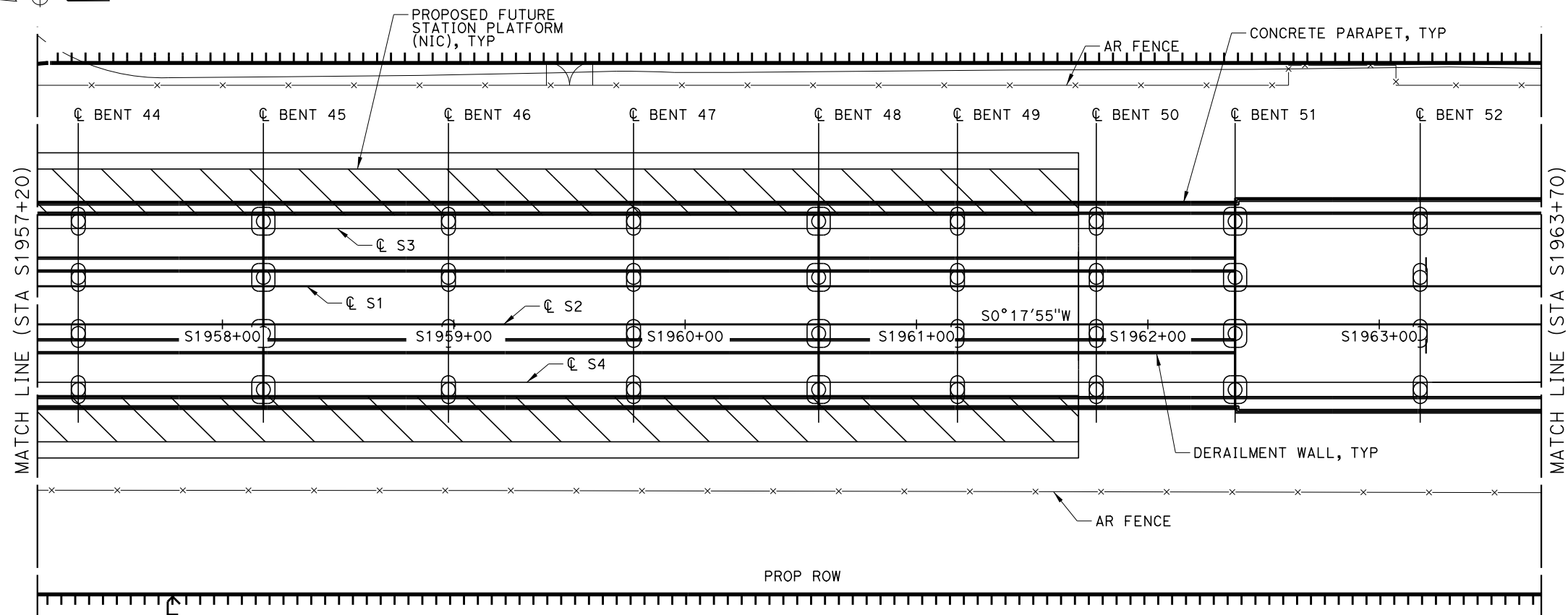
USL450-APP064\$

NOTES:

- FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.

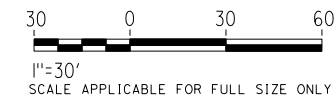


ELEVATION
SCALE: 1" = 30'



PLAN
SCALE: 1" = 30'

TO FRESNO
TO BAKERSFIELD



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
M. LEWIS

DRAWN BY
M. SPURGEON

CHECKED BY
S. VARELA

IN CHARGE
R. TREVETT

DATE
04/06/20



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3

SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
6 OF 13

CONTRACT NO.
HSR 13-57

DRAWING NO.
ST-J1008-HNV

SCALE
AS SHOWN

SHEET NO.
11 OF 66

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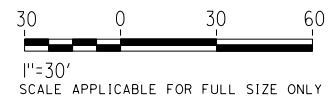
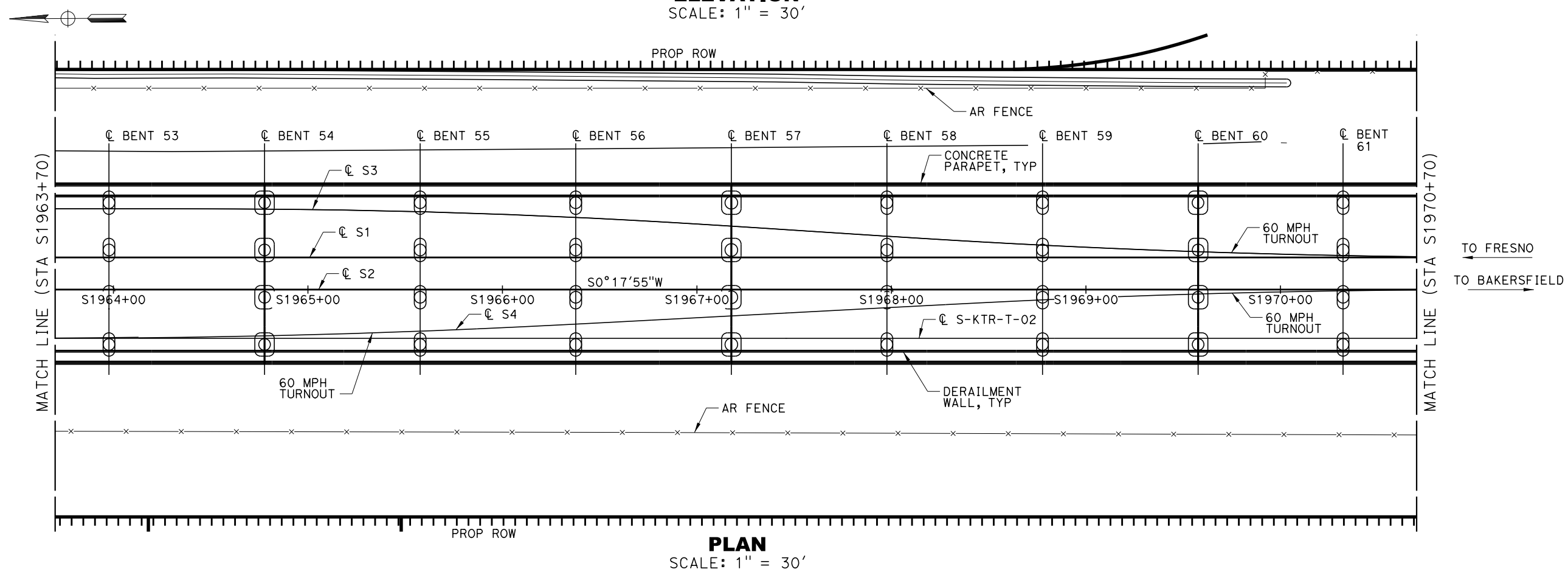
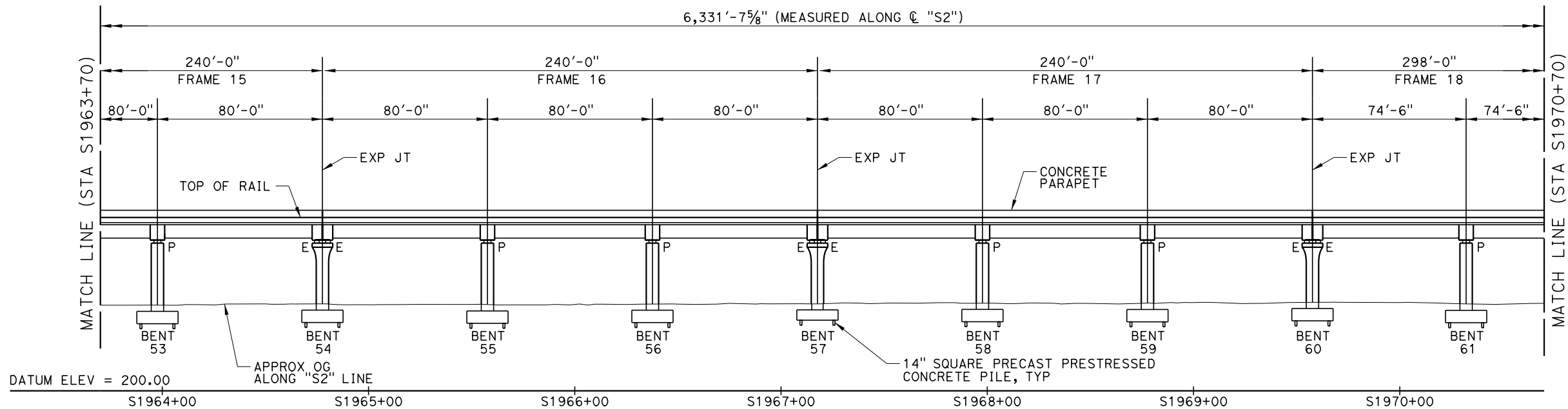
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4/3/2020 1:02:19 PM

USL450-APP064\$

NOTES:

1. FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
R. TREVETT
DATE
04/06/20



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3
SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
7 OF 13

CONTRACT NO. HSR 13-57
DRAWING NO. ST-J1009-HNV
SCALE AS SHOWN
SHEET NO. 12 OF 66

D:\NCS_workdir\56421\503262_633\1357-235-ST-J1010-HNV.dgn

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7/24/2020 10:11:54 AM

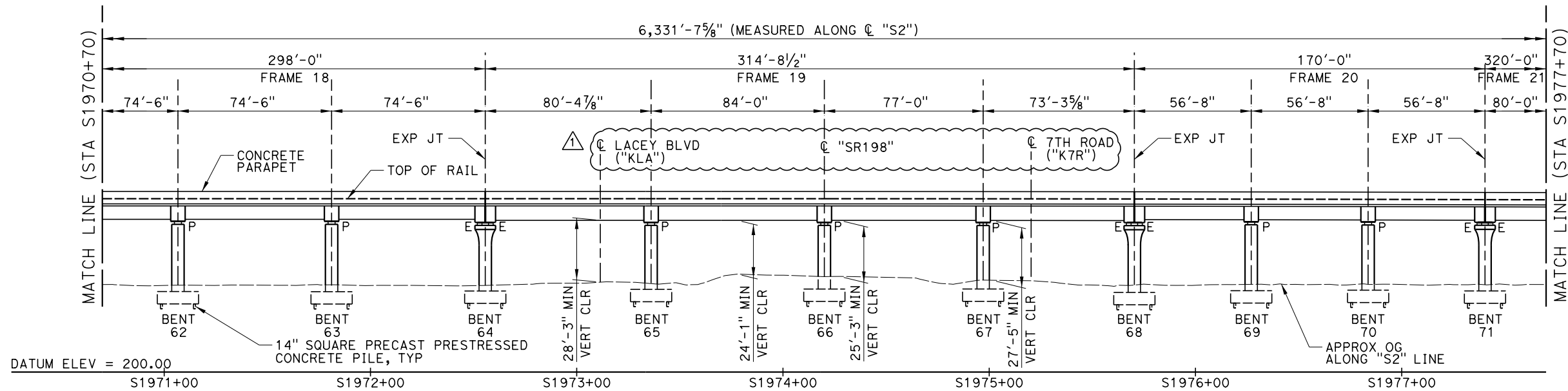
USL450-APP064\$

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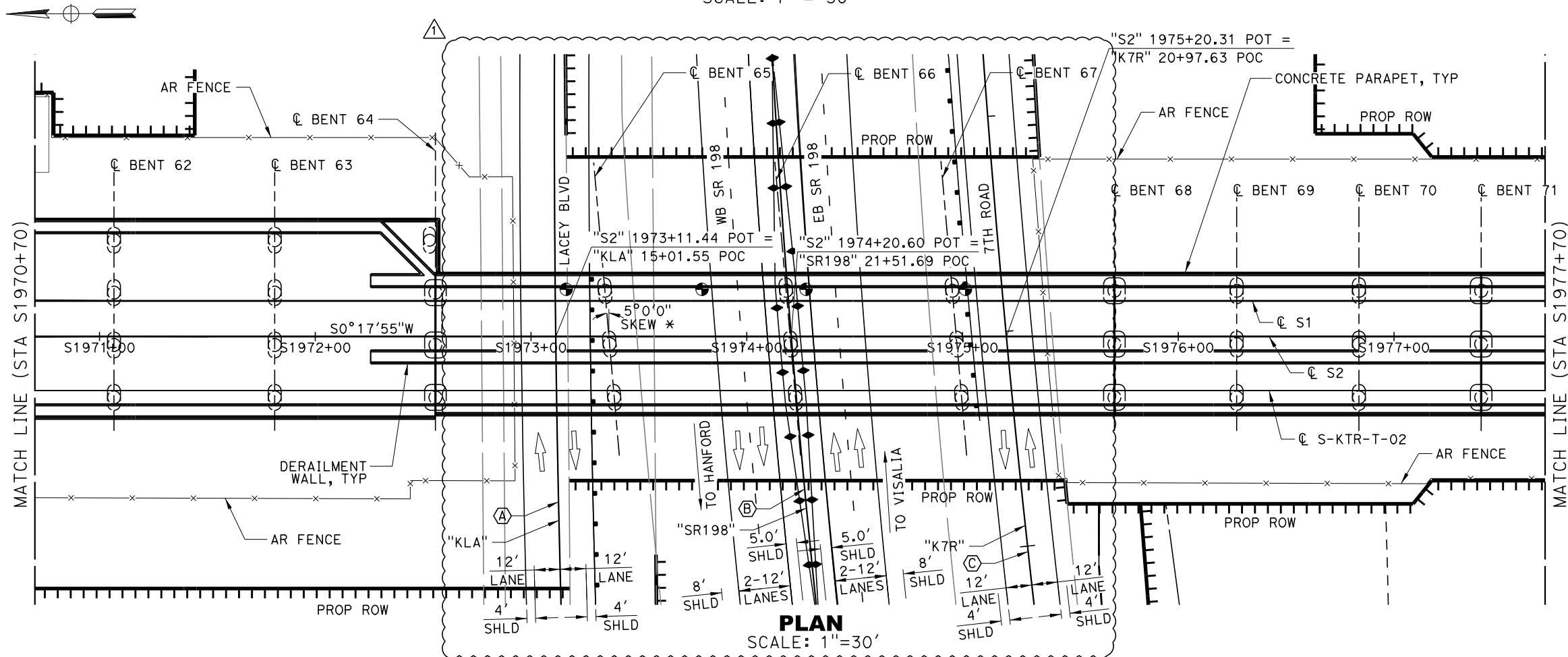
NOTES:

- FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.

* BENT 65 THRU 67 ONLY



ELEVATION
SCALE: 1" = 30'



PLAN
SCALE: 1" = 30'

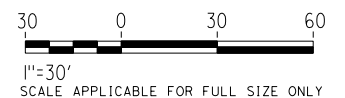
CURVE DATA

(A) "KLA" LINE
R = 8370.00'
Δ = 02°53'33"
T = 211.33'
L = 422.56'

(B) "SR198" LINE
R = 5072.00'
Δ = 16°06'07"
T = 717.43'
L = 1425.40'

(C) "K7R" LINE
R = 6220.00'
Δ = 14°07'06"
T = 770.24'
L = 1532.69'

TO FRESNO
TO BAKERSFIELD



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION
1	7/24/20	ML	JB	GF	DESIGN UPDATE

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
G. FROMM
DATE
07/24/2020



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3

SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
8 OF 15

CONTRACT NO.
HSR 13-57
DRAWING NO.
ST-J1010-HNV
SCALE
AS SHOWN
SHEET NO.
15 OF 279

D:\ICS_workdir\49712\503262_9\1357-235-ST-J1011-HNV.dgn

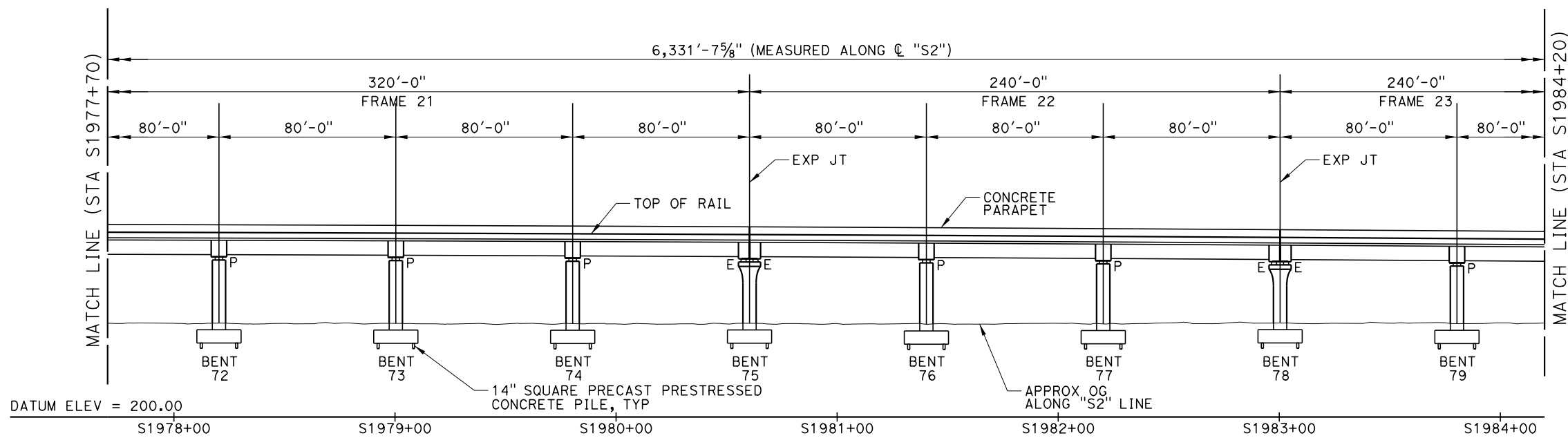
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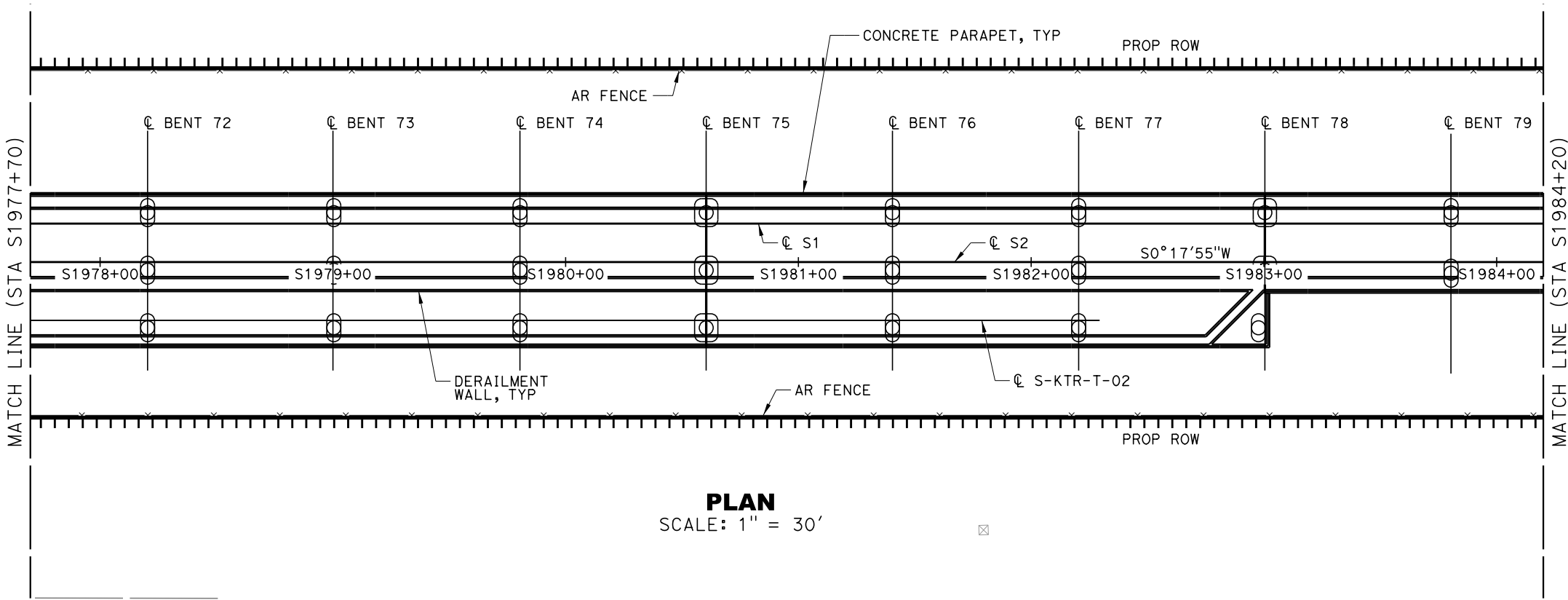
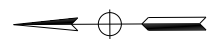
USLASO-APP064\$

NOTES:

1. FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.

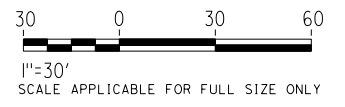


ELEVATION
SCALE: 1" = 30'



PLAN
SCALE: 1" = 30'

TO FRESNO
TO BAKERSFIELD



1"=30'
SCALE APPLICABLE FOR FULL SIZE ONLY

FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
M. LEWIS

DRAWN BY
M. SPURGEON

CHECKED BY
S. VARELA

IN CHARGE
R. TREVETT

DATE
04/06/20



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3

SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
9 OF 13

CONTRACT NO.
HSR 13-57

DRAWING NO.
ST-J1011-HNV

SCALE
AS SHOWN

SHEET NO.
14 OF 66

D:\ICS_workdir\49712\503262_10\1357-235-ST-J1012-HNV.dgn

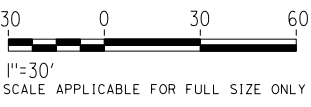
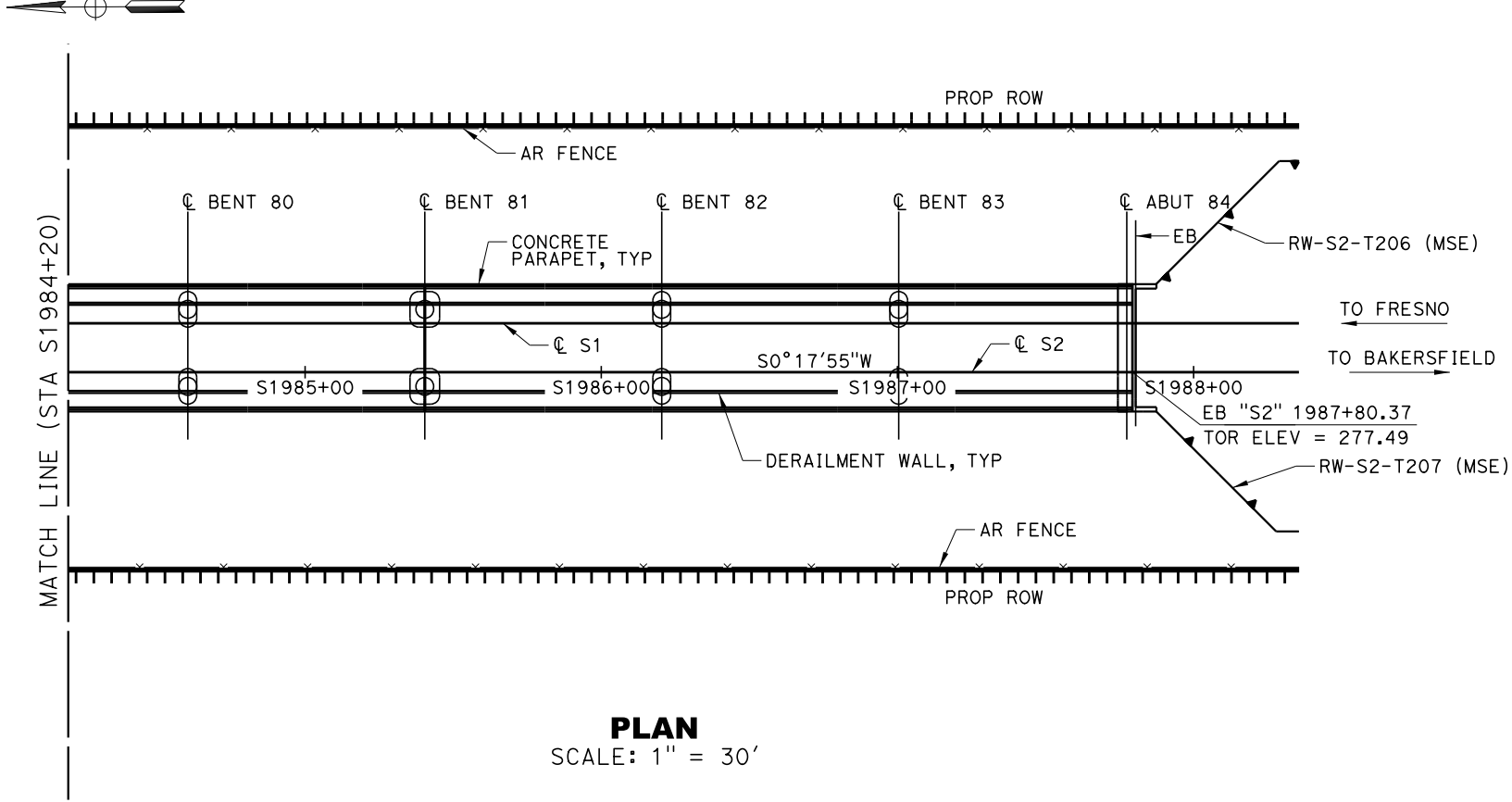
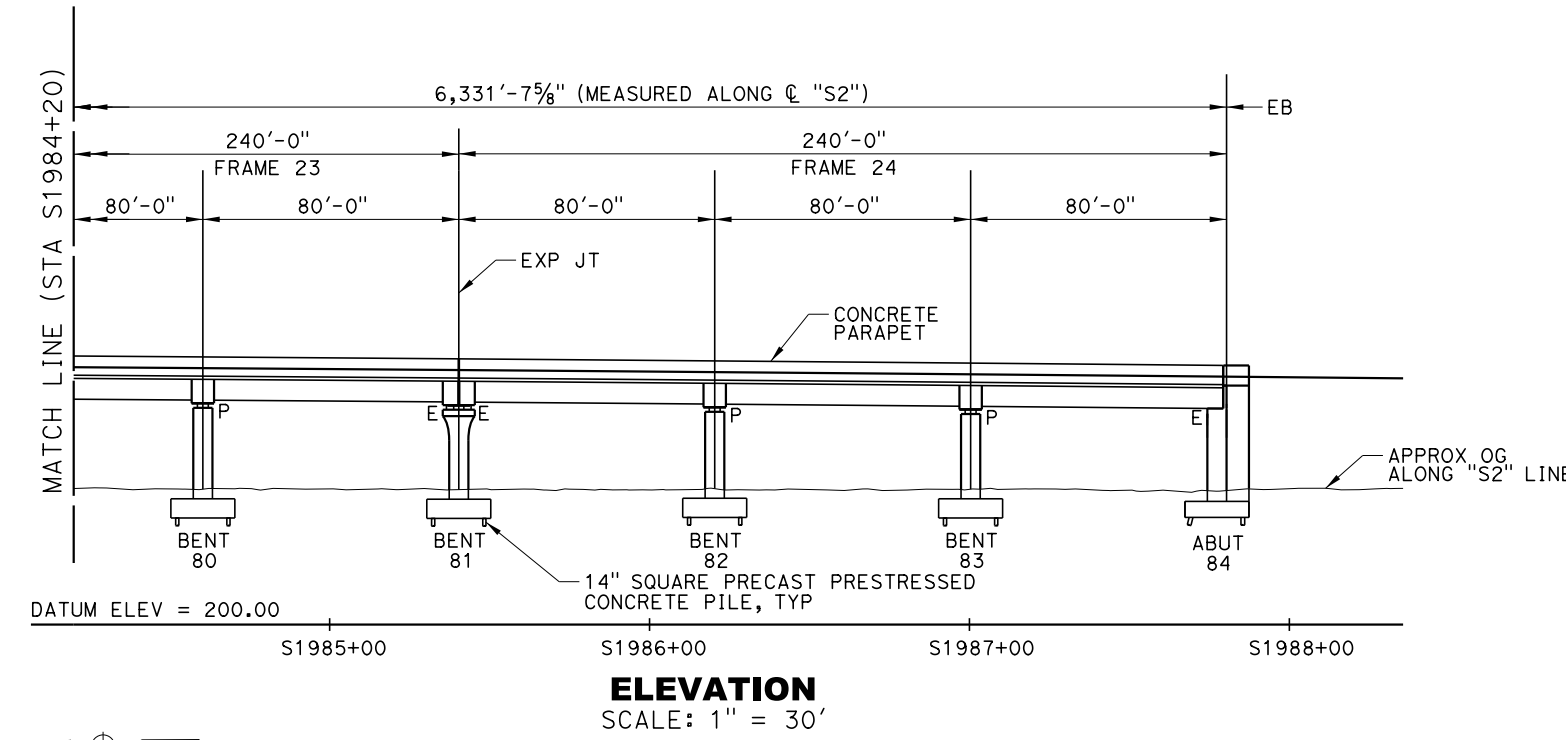
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4/3/2020 1:02:14 PM

USLA50-APP064\$

NOTES:

1. FOR NOTES, LEGEND AND ALIGNMENT DATA, SEE DRAWING NO. ST-J1003-HNV.



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION

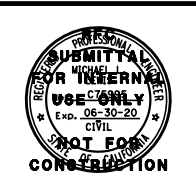
DESIGNED BY
M. LEWIS

DRAWN BY
M. SPURGEON

CHECKED BY
S. VARELA

IN CHARGE
R. TREVETT

DATE
04/06/20



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3

SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
10 OF 13

CONTRACT NO.
HSR 13-57

DRAWING NO.
ST-J1012-HNV

SCALE
AS SHOWN

SHEET NO.
15 OF 66

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7/24/2020 10:12:20 AM

USL450-APP064\$

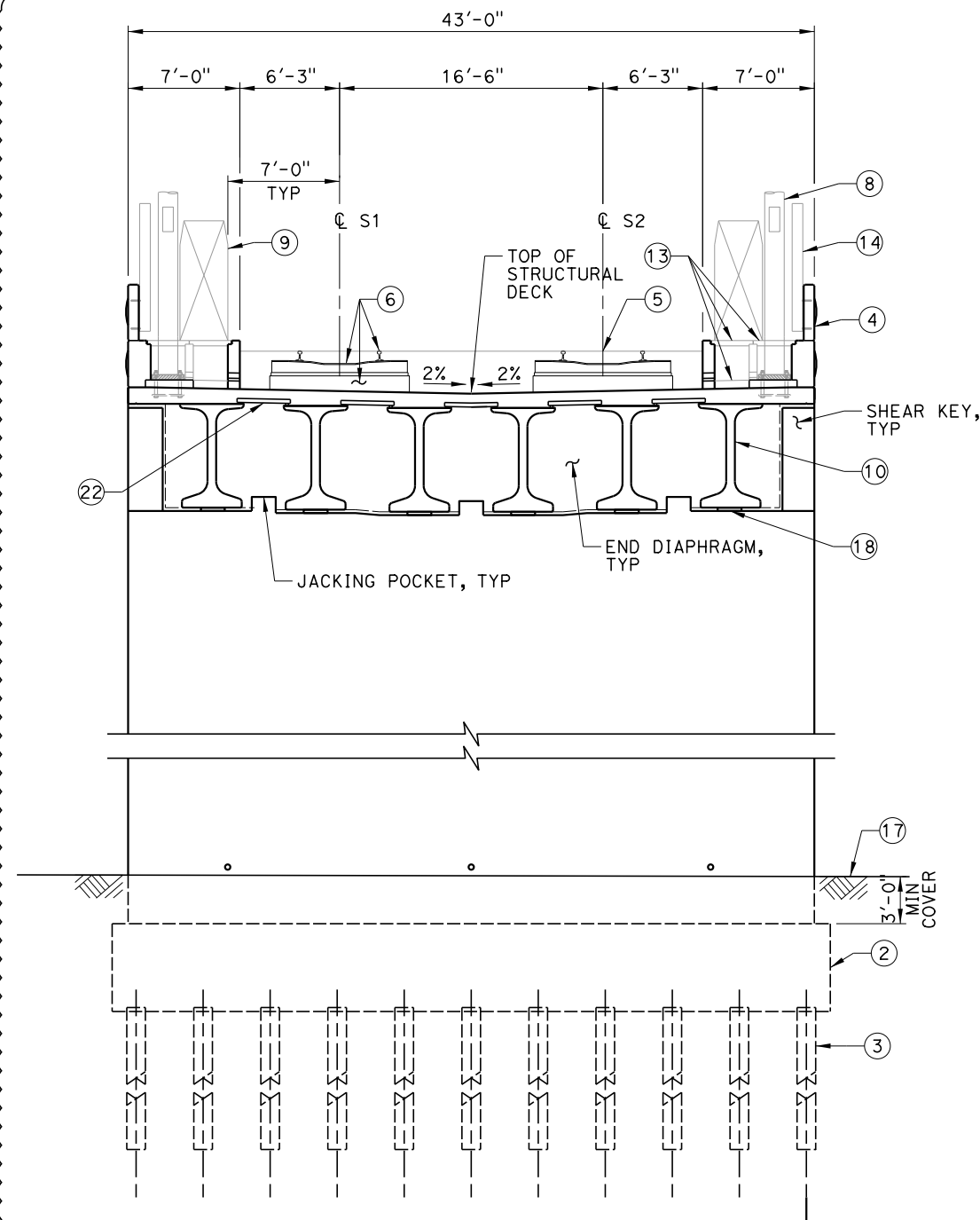
THIS SHEET IS PART OF AN EARLY RELEASE CONSTRUCTION PACKAGE, WHICH IS BEING CONSTRUCTED AT THE CONTRACTOR'S RISK. THIS SHEET SHALL BE DISREGARDED AS SOON AS THE RFC PACKAGE FOR THESE SHEETS HAVE BEEN APPROVED BY THE AUTHORITY AND ISSUED FOR CONSTRUCTION. DFJV ACCEPTS ALL RESPONSIBILITY AND LIABILITY FOR ANY CONSTRUCTION CHANGES THAT OCCUR PRIOR TO FINAL APPROVAL OF THE RELEASE FOR CONSTRUCTION PACKAGE.

NOTES:

- THE FOLLOWING ITEMS ARE FOR ILLUSTRATION PURPOSES ONLY AND NOT IN CONTRACT (NIC): SOUNDWALLS, OCS POLES, CABLE TROUGHS, PRECAST COVERS, DIRECT FIXATION RAIL SYSTEM TRACK SLAB.
- STRUCTURE DEPTH IS MEASURED TO THE TOP OF THE STRUCTURAL DECK AT THE GIRDER END ALONG ϕ OF GIRDER.

LEGEND:

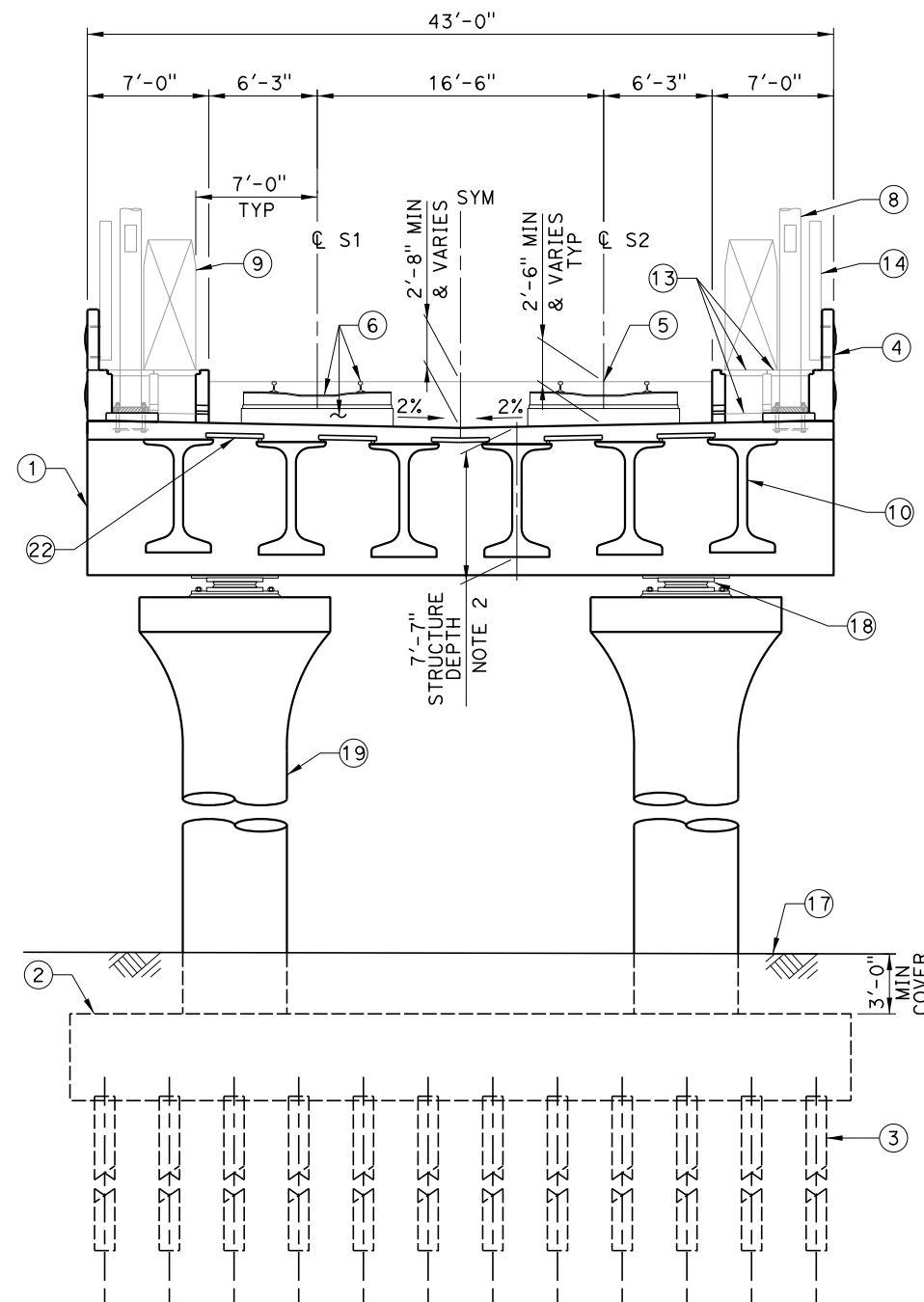
- ① BENT CAP
- ② PILE CAP
- ③ 14" SQUARE PRECAST PRESTRESSED CONCRETE PILE
- ④ CONCRETE PARAPET
- ⑤ TOP OF RAIL "S2" LINE
- ⑥ DIRECT - FIXATED RAIL SYSTEM (NIC)
- ⑦ OCS PORTAL STRUCTURE (NIC)
- ⑧ OCS POLE (NIC)
- ⑨ 3'-0"x7'-6" WALKWAY CLEARANCE ENVELOPE
- ⑩ CA WF78 PRECAST/PRESTRESSED CONCRETE GIRDER
- ⑪ CA WF84 PRECAST/PRESTRESSED CONCRETE GIRDER
- ⑫ CA WF120 PRECAST/PRESTRESSED CONCRETE GIRDER
- ⑬ CABLE TROUGH WITH PRECAST COVERS (NIC)
- ⑭ FUTURE SOUNDWALL (NIC)
- ⑮ FUTURE STATION PLATFORM (NIC)
- ⑯ COLUMN ISOLATION CASING (BENTS 64-69 ONLY)
- ⑰ FINISHED GRADE
- ⑱ BEARING
- ⑲ 72"Ø COLUMN W/ ARCHITECTURAL FLARE
- ⑳ 78"Ø COLUMN W/ ARCHITECTURAL FLARE
- ㉑ 108"Ø CIDH
- ㉒ PRECAST DECK PANEL



SECTION AT ABUTMENT

SCALE: $\frac{3}{16}$ " = 1'-0"

ABUTMENT 84 SHOWN, ABUTMENT 1 SIMILAR



TYPICAL SECTION (FRAMES 1, 23 & 24)

SCALE: $\frac{3}{16}$ " = 1'-0"

FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION
1	7/24/20	ML	JB	GF	DESIGN UPDATE

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
G. FROMM
DATE
07/24/2020



**CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3**

SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
11 OF 15

CONTRACT NO.	HSR 13-57
DRAWING NO.	ST-J1013-HNV
SCALE	AS SHOWN
SHEET NO.	18 OF 279

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ip1otdr-vm.plt

CAHSR RFC_SIGNED.tbl

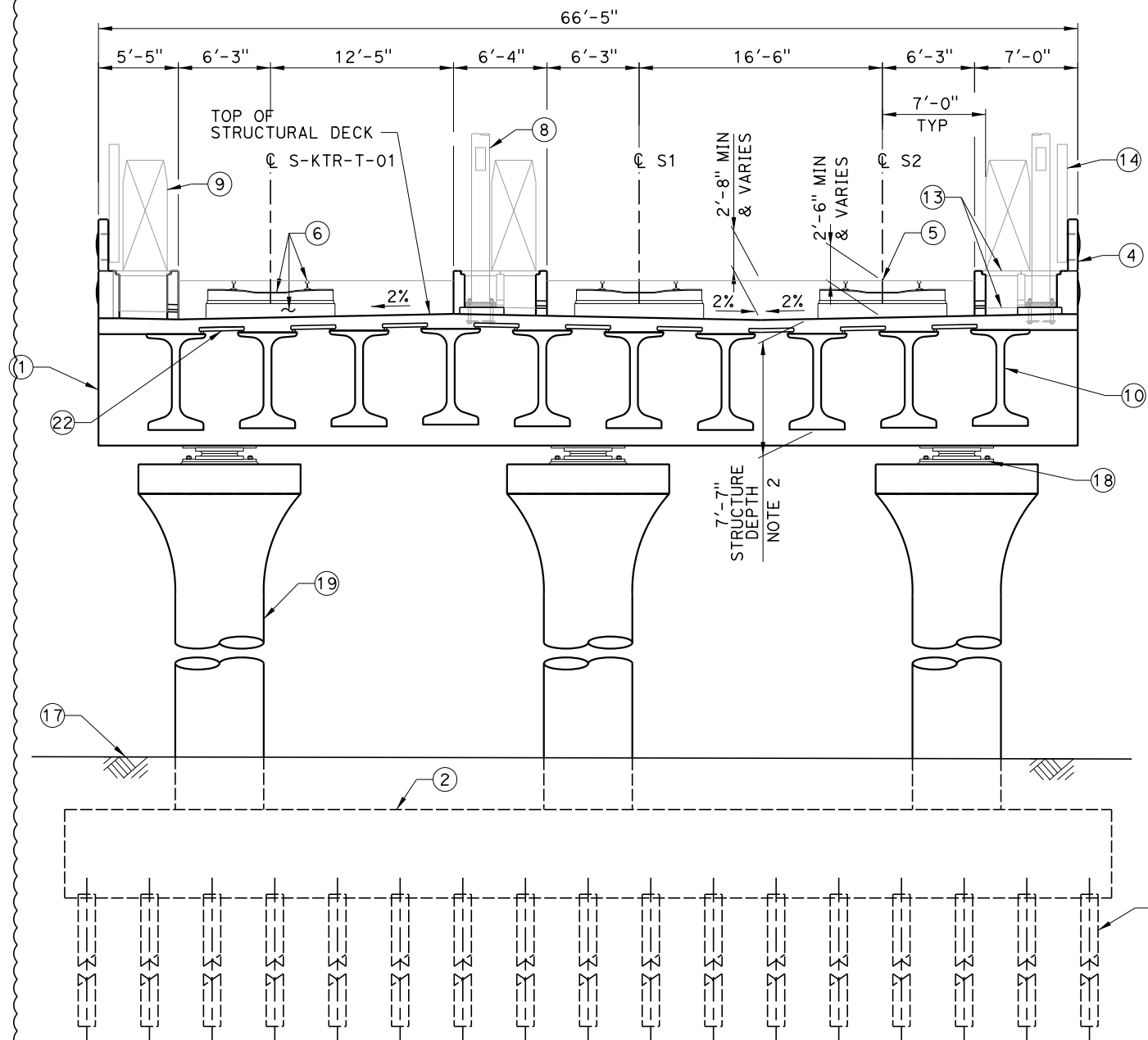
7/24/2020 10:12:30 AM

USL450-APP064\$

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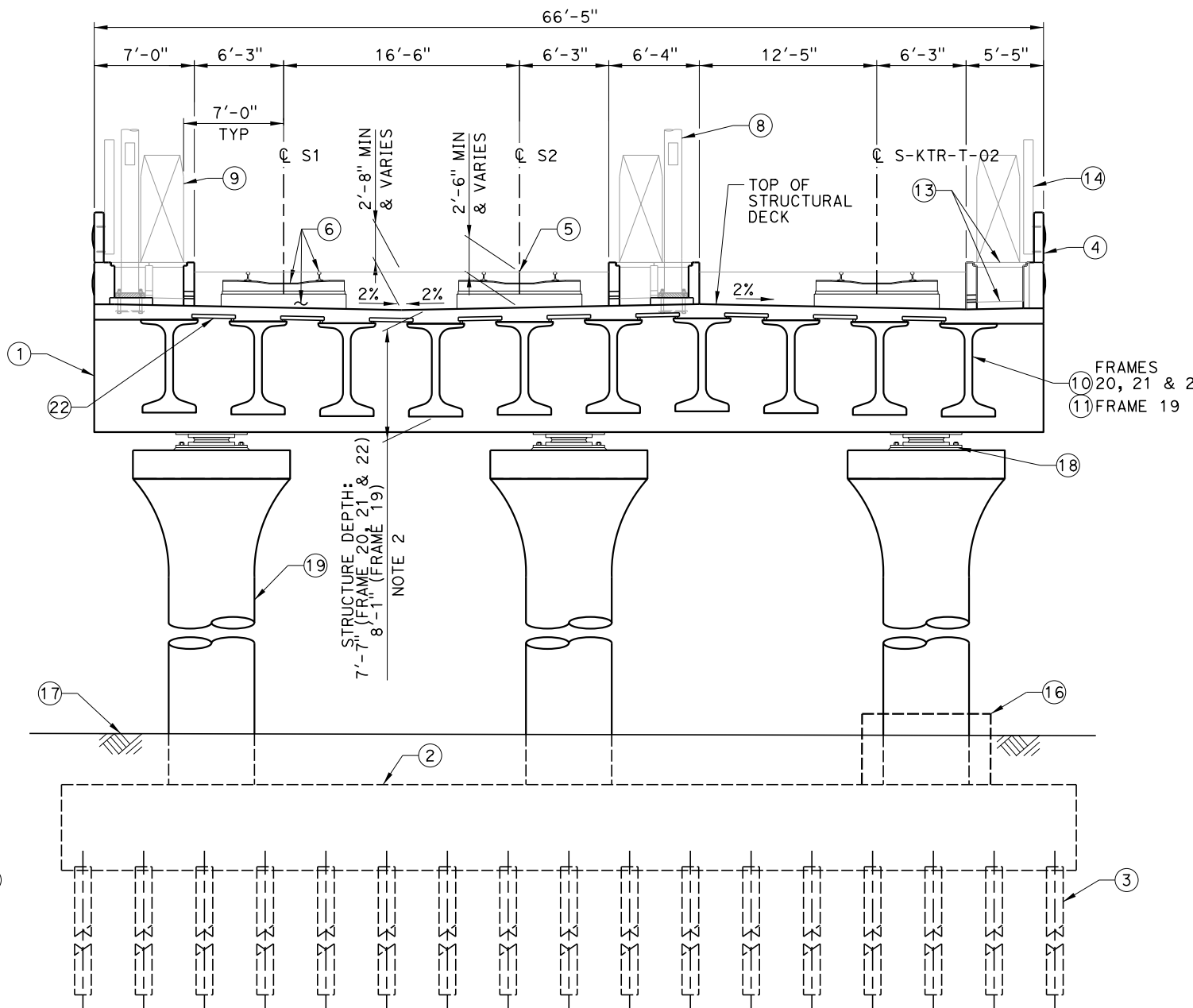
NOTES:

1. FOR NOTES AND LEGEND, SEE DRAWING NO. ST-J1013-HNV.



TYPICAL SECTION (FRAMES 2, 3, & 4)

SCALE: $\frac{3}{16}" = 1'-0"$



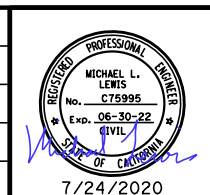
TYPICAL SECTION (FRAMES 20, 21 & 22; FRAME 19 SIMILAR)

SCALE: $\frac{3}{16}" = 1'-0"$

FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION
1	7/24/20	ML	JB	GF	DESIGN UPDATE

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
G. FROMM
DATE
07/24/2020



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3
SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
12 OF 15

CONTRACT NO.
HSR 13-57
DRAWING NO.
ST-J1014-HNV
SCALE
AS SHOWN
SHEET NO.
19 OF 279

D:\ICS_workdir\56421\503262_23\1357-23S-ST-J1015-HNV.dgn

CAHSRP_RFC_SIGNED.tbi iplotdrvm.plt

7/24/2020 10:12:39 AM

USLASO-APP064\$

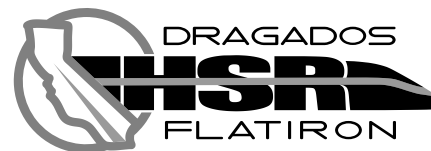
1. FOR NOTES AND LEGEND, SEE DRAWING NO.
ST-J1013-HNV.



SCALE: $\frac{3}{16}'' = 1'-0''$

1	7/24/20	ML	JB	GF	DESIGN UPDATE
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY M. LEWIS
DRAWN BY M. SPURGEON
CHECKED BY S. VARELA
IN CHARGE G. FROMM
DATE 07/24/202



SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
13 OF 15

CONTRACT NO.	HSR 13-57
DRAWING NO.	ST-J1015-HNV
SCALE	AS SHOWN
SHEET NO.	20 OF 279

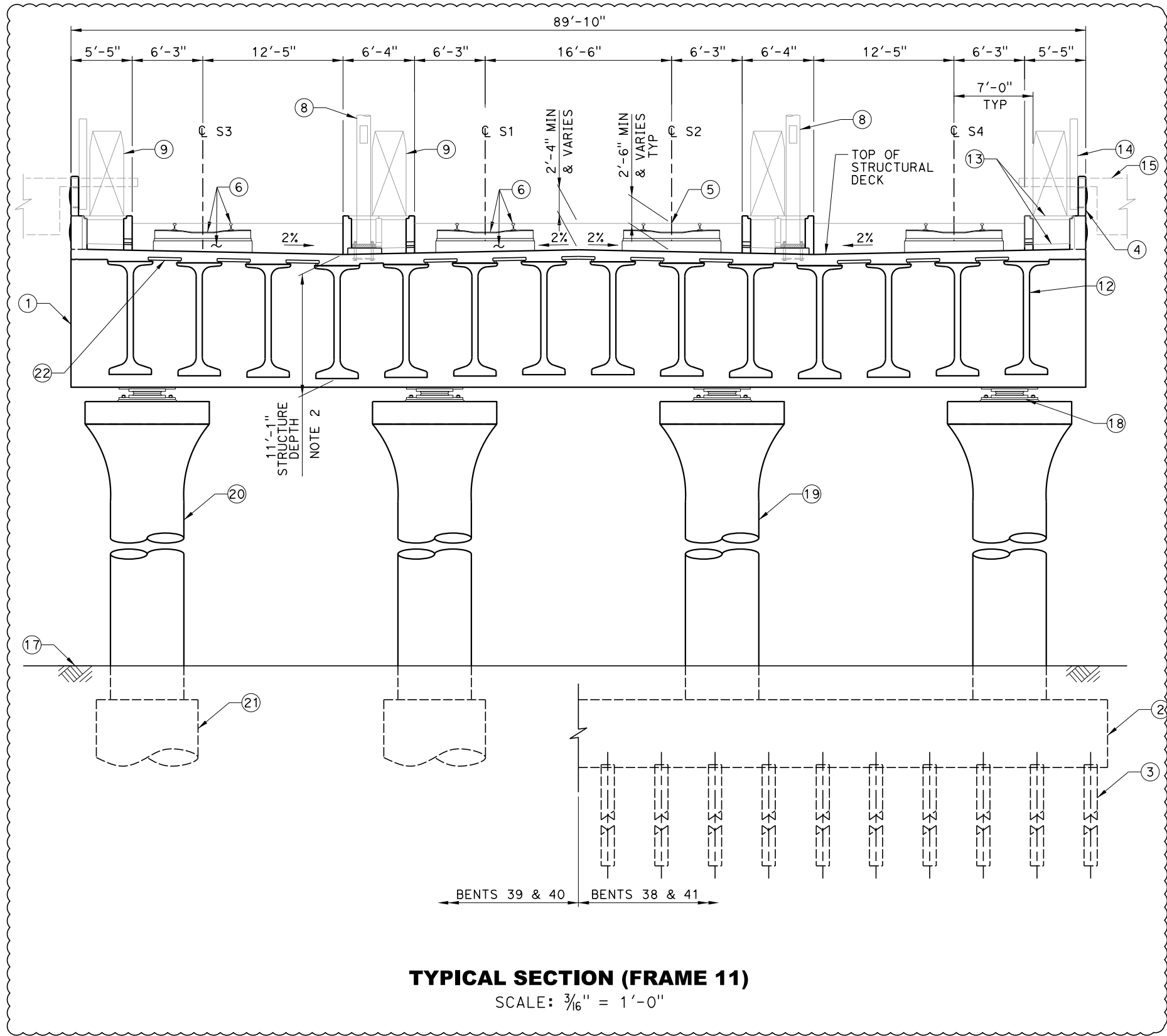
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NOTES:

1. FOR NOTES AND LEGEND, SEE DRAWING NO. ST-J1013-HNV.

THIS SHEET IS PART OF AN EARLY RELEASE CONSTRUCTION PACKAGE, WHICH IS BEING CONSTRUCTED AT THE CONTRACTOR'S RISK. THIS SHEET SHALL BE DISREGARDED AS SOON AS THE RFC PACKAGE FOR THESE SHEETS HAVE BEEN APPROVED BY THE AUTHORITY AND ISSUED FOR CONSTRUCTION. DFJV ACCEPTS ALL RESPONSIBILITY AND LIABILITY FOR ANY CONSTRUCTION CHANGES THAT OCCUR PRIOR TO FINAL APPROVAL OF THE RELEASE FOR CONSTRUCTION PACKAGE.

FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION
1	7/24/20	ML	JB	GF	DESIGN UPDATE

DESIGNED BY
M. LEWIS
DRAWN BY
M. SPURGEON
CHECKED BY
S. VARELA
IN CHARGE
G. FROMM
DATE
07/24/2020



**CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3**
SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
14 OF 15

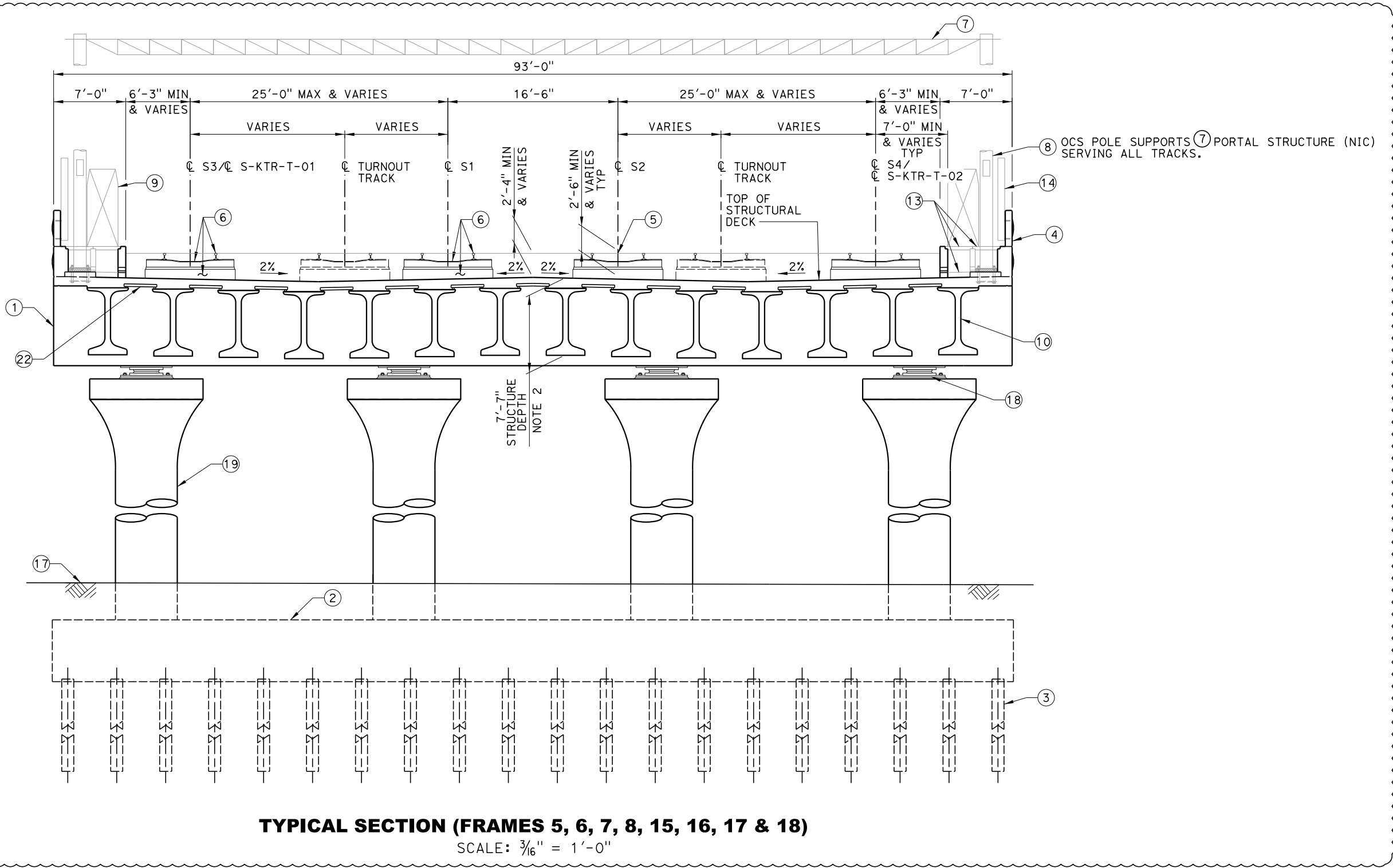
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THIS SHEET IS PART OF AN EARLY RELEASE CONSTRUCTION PACKAGE, WHICH IS BEING CONSTRUCTED AT THE CONTRACTOR'S RISK. THIS SHEET SHALL BE DISREGARDED AS SOON AS THE RFC PACKAGE FOR THESE SHEETS HAVE BEEN APPROVED BY THE AUTHORITY AND ISSUED FOR CONSTRUCTION. DFJV ACCEPTS ALL RESPONSIBILITY AND LIABILITY FOR ANY CONSTRUCTION CHANGES THAT OCCUR PRIOR TO FINAL APPROVAL OF THE RELEASE FOR CONSTRUCTION PACKAGE.

NOTES:

1. FOR NOTES AND LEGEND, SEE DRAWING NO. ST-J1013-HNV.



FID: S-AS-223.0

REV	DATE	BY	CHK	APP	DESCRIPTION
1	7/24/20	ML	JB	GF	DESIGN UPDATE

DESIGNED BY M. LEWIS
DRAWN BY M. SPURGEON
CHECKED BY S. VARELA
IN CHARGE G. FROMM
DATE 07/24/2020



**CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3**
SEGMENT 2
HANFORD VIADUCT
GENERAL PLAN AND ELEVATION
15 OF 15

CONTRACT NO. HSR 13-57
DRAWING NO. ST-J1017-HNV
SCALE AS SHOWN
SHEET NO. 22 OF 279

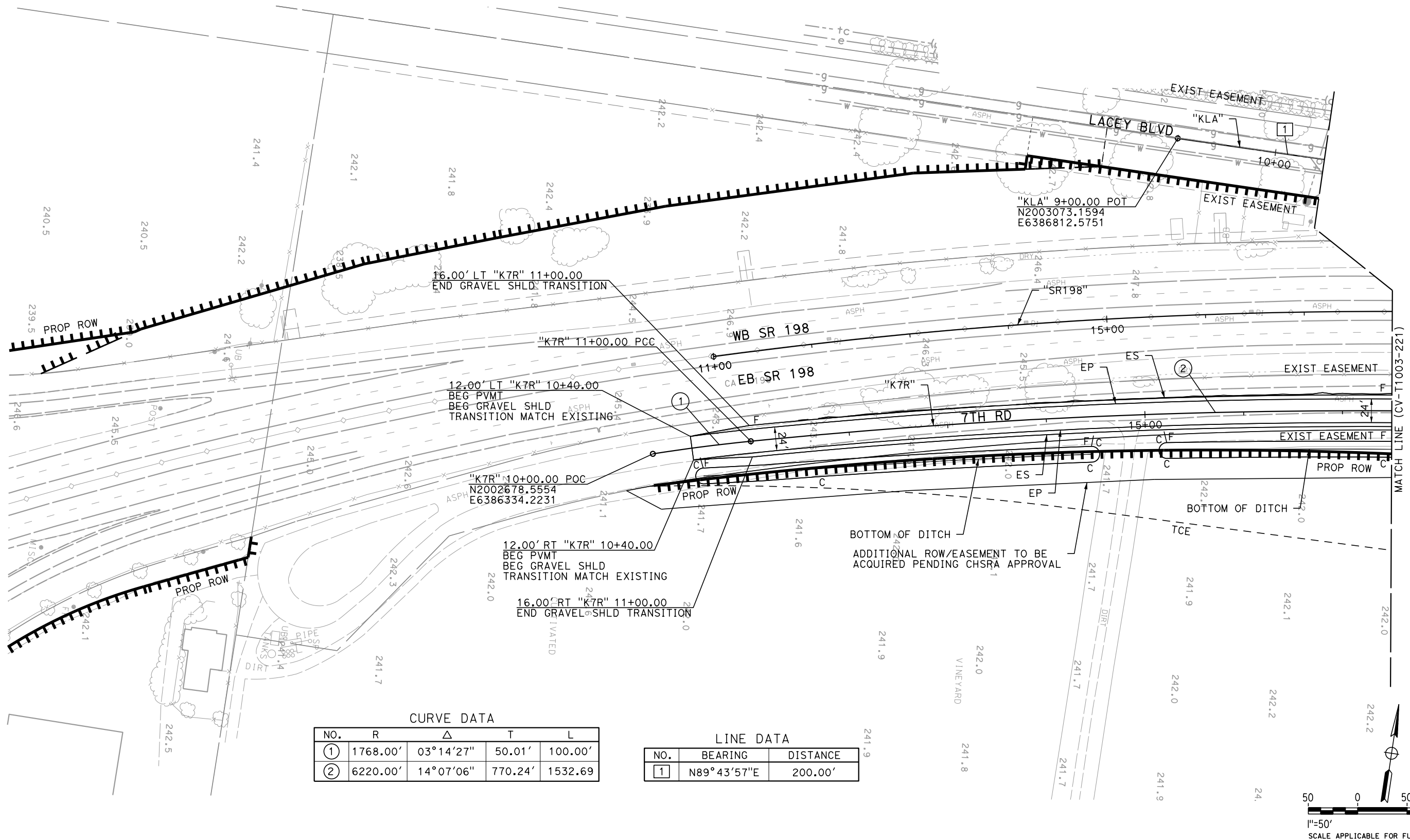
EXHIBIT E1

Profile Plan Drawings

7th Road underpass crossing

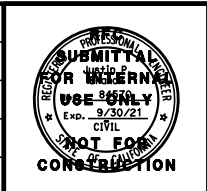
**Dragados Flatiron Joint Venture
1775 Park Street Suite 75
Selma, CA 93662**

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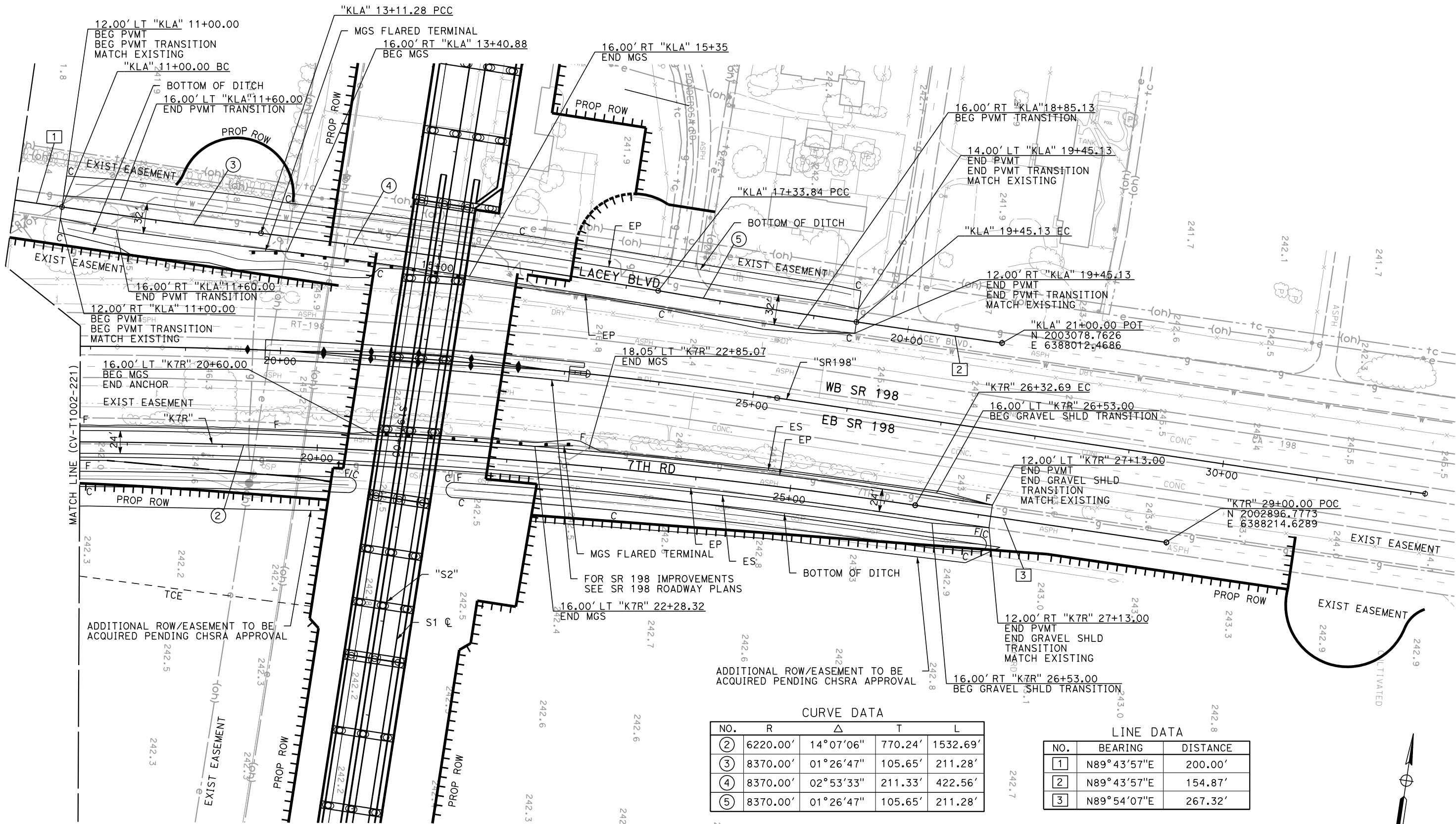
REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
J. BRANCH
DRAWN BY
K. NGUYEN
CHECKED BY
D. SANCHEZ
IN CHARGE
G. FROMM
DATE
04/17/2020



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3
ROADWAY SEGMENT 2
GRADE SEPARATION LAYOUT
7TH RD & LACEY BLVD

CONTRACT NO.
HSR 13-57
DRAWING NO.
CV-T1002-221
SCALE
AS SHOWN
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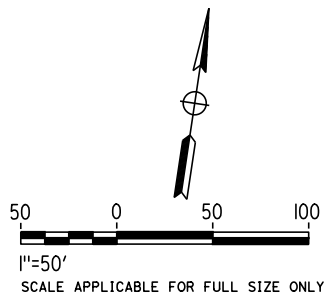


CURVE DATA

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③	8370.00'	01°26'47"	105.65'	211.28'
④	8370.00'	02°53'33"	211.33'	422.56'
⑤	8370.00'	01°26'47"	105.65'	211.28'

LINE DATA

NO.	BEARING	DISTANCE
①	N89°43'57"E	200.00'
②	N89°43'57"E	154.87'
③	N89°54'07"E	267.32'



REV	DATE	BY	CHK	APP	DESCRIPTION

DESIGNED BY
J. BRANCH
DRAWN BY
K. NGUYEN
CHECKED BY
D. SANCHEZ
IN CHARGE
G. FROMM
DATE
04/17/2020

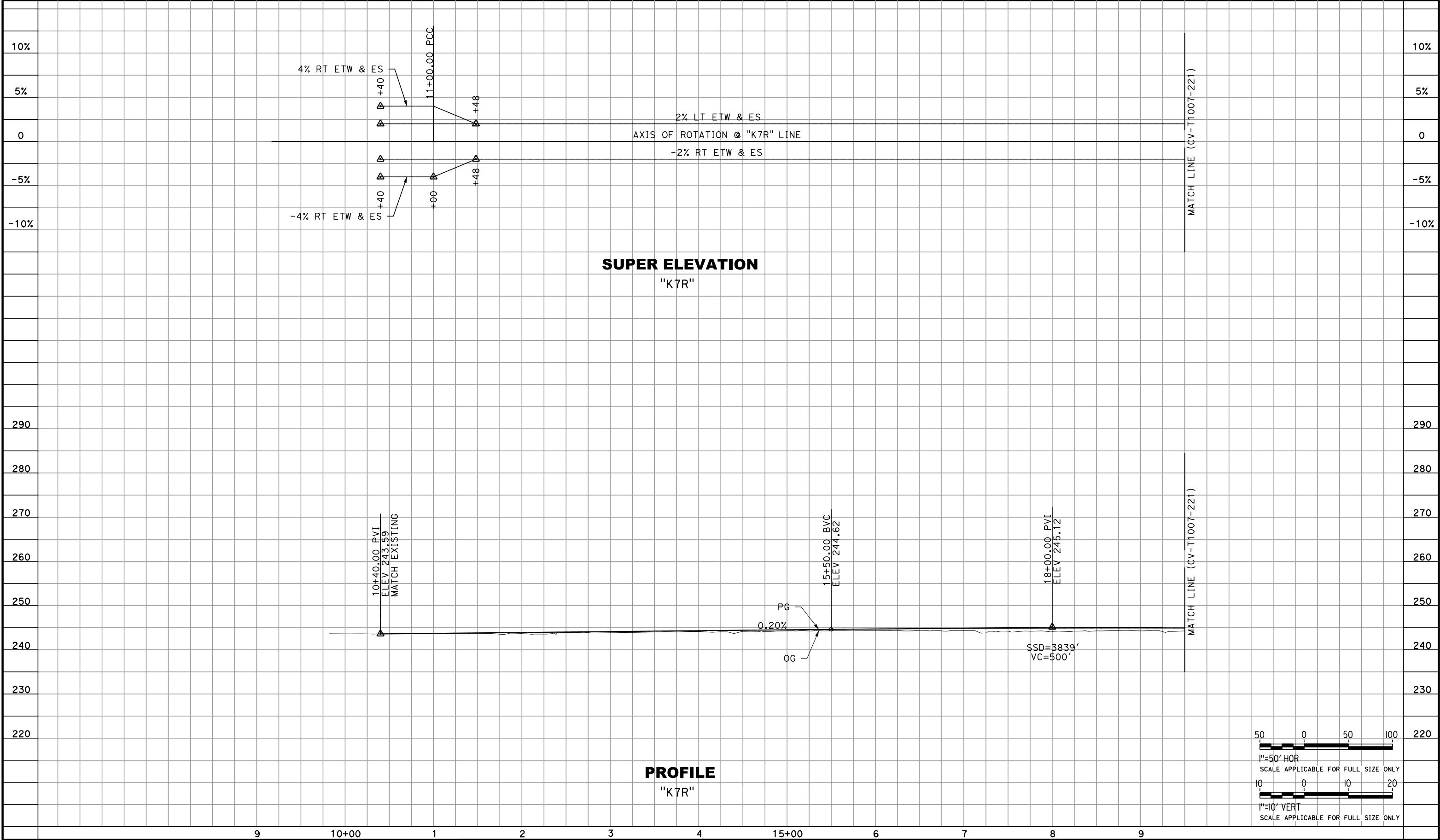


**CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3**

ROADWAY SEGMENT 2
GRADE SEPARATION LAYOUT
7TH RD & LACEY BLVD

CONTRACT NO.
HSR 13-57
DRAWING NO.
CV-T1003-221
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						DESIGNED BY J. BRANCH				CALIFORNIA HIGH-SPEED TRAIN PROJECT FB - CONSTRUCTION PACKAGE 2-3 ROADWAY SEGMENT 2 GRADE SEPARATION PROFILE AND SUPERELEVATION 7TH RD	CONTRACT NO. HSR 13-57
						DRAWN BY K. NGUYEN					DRAWING NO. CV-T1006-221
						CHECKED BY J. BRANCH					SCALE AS SHOWN
						IN CHARGE G. FROMM					SHEET NO. OF
REV	DATE	BY	CHK	APP	DESCRIPTION	DATE 04/17/2020					



EXHIBIT F1

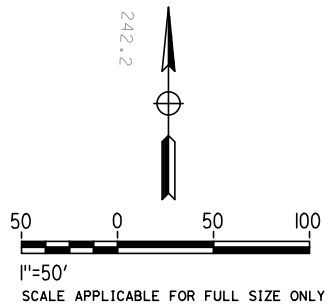
Pavement Delineation and Sign Plan

7th Road underpass crossing

Dragados Flatiron Joint Venture

1775 Park Street Suite 75

Selma, CA 93662



DESIGNED BY	K. NGUYEN
DRAWN BY	K. NGUYEN
CHECKED BY	J. BRANCH
IN CHARGE	G. FROMM
DATE	04/17/2020



CALIFORNIA HIGH-SPEED TRAIN PROJECT
FB - CONSTRUCTION PACKAGE 2-3
 ROADWAY SEGMENT 2
 PAVEMENT DELINEATION AND SIGN PLAN
 7TH RD

CONTRACT NO.	HSR 13-57
DRAWING NO.	CV-I1302-221
SCALE	AS SHOWN
SHEET NO.	OF

EXHIBIT G1

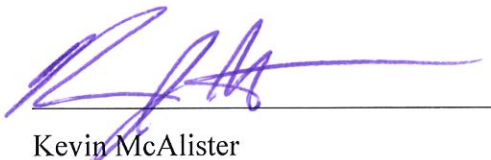
Local Project Support

Concurrence Letter from Kings County

**Dragados Flatiron Joint Venture
1775 Park Street Suite 75
Selma, CA 93662**

7th Road Underpass Structure, CHSRA
Milepost 223.96

I, Kevin McAlister, an employee of County of Kings, authorized to sign this letter of agreement on its behalf, hereby declare that County of Kings concurs with CHSRA's request to construct one new grade separation at 7th Road, Hanford Viaduct (223.96). Approval of the final design and construction of any new grade separation shall follow the applicable agreements entered into by County of Kings and CHSRA.



Kevin McAlister
Kings County Public Works Director
County of Kings
1400 W. Lacey Blvd
Hanford, CA 93230

7/29/2020

Date

EXHIBIT G2

Local Project Support

Concurrence Letter from City of Hanford

Dragados Flatiron Joint Venture

1775 Park Street Suite 75

Selma, CA 93662

7th Road Underpass Structure, CHSRA
Milepost 223.96

I, Johnathan Doyel, an employee of City of Hanford, authorized to sign this letter of agreement on its behalf, hereby declare that City of Hanford concurs with the proposed CP2-3 project at 7th Road, Hanford Viaduct (223.96).



Johnathan Doyel
City of Hanford Public Works Director
900 S 10th Ave,
Hanford, CA 93230

09/08/2020

Date

EXHIBIT H

Verification for Application

**Dragados Flatiron Joint Venture
1775 Park Street Suite 75
Selma, CA 93662**

Exhibit H

Verification

I am an officer of the California High-Speed Rail Authority, Applicant herein, and am authorized to make this verification on its behalf. The contents of this document are true of my own knowledge, except as matters that are stated on information or belief, and as to those matters, I believe them to be true.

Dated this 02 day of February, ²⁰²¹~~2020~~ at Sacramento, California.

Signed _____



Bruce W. Armistead
Director of Operations and Maintenance
California High-Speed Rail Authority
770 L Street, Suite 620
Sacramento, CA 95814
Tel: (916) 324-1541

EXHIBIT I

Notice of Availability

Final EIR / EIS Fresno to Bakersfield Section of California High-Speed Train Project

**Dragados Flatiron Joint Venture
1775 Park Street Suite 75
Selma, CA 93662**

BEFORE THE PUBLIC UTILITIES COMMISSION OF THE STATE OF CALIFORNIA

Application of the California High-Speed Rail
Authority for Approval to Construct One
Underpass Grade-Separated Crossing at 7th
Road (MP 223.96), Under Two Proposed High-
Speed Rail Tracks As Part of the Proposed
High-Speed Rail Grade-Separated Structure,
Hanford Viaduct, Located in the County of
Kings, State of California.

Application No. _____

NOTICE OF AVAILABILITY

**FINAL ENVIRONMENTAL IMPACT REPORT / ENVIRONMENTAL IMPACT
STATEMENT FOR THE FRESNO TO BAKERSFIELD SECTION OF THE CALIFORNIA
HIGH-SPEED TRAIN PROJECT**

TO ALL PARTIES TO THIS APPLICATION:

In support of its applications, the California High-Speed Rail Authority (Applicant) submitted the *Final Environmental Impact Report / Environmental Impact Statement for the Fresno to Bakersfield Section of the California High-Speed Train Project (Final EIR / EIS)* on an archival CD-ROM/DVD and six copies on standard CD-ROM/DVD to the Docket Office for physical filing as Exhibit I to application A1709017.

Pursuant to Rule 1.9(d) of the Commission's Rules of Practice and Procedure, the Applicant is issuing this Notice of Availability (NOA). The NOA is being served on all parties listed on the official service lists for this application, which are attached as Exhibit K – Certificate of Service.

The Final EIR/EIS is available at the following URL, which has been posted on the web since 2014: http://www.hsr.ca.gov/Programs/Environmental_Planning/final_fresno_bakersfield.html

California High Speed Rail Authority
Bruce Armistead
Director of Operations and Maintenance
Sacramento, CA 95814
Bruce.Armistead@hsr.ca.gov

EXHIBIT J1

Scoping Memo

7th Road underpass crossing

Dragados Flatiron Joint Venture

1775 Park Street Suite 75

Selma, CA 93662

Scoping Memo Information for Applications

A. Category (Check the category that is most appropriate)

☐ Adjudicatory - “Adjudicatory” proceedings are: (1) enforcement investigations into possible violations of any provision of statutory law or order or rule of the Commission; and (2) complaints against regulated entities, including those complaints that challenge the accuracy of a bill, but excluding those complaints that challenge the reasonableness of rates or charges, past, present, or future, such as formal rough crossing complaints (maximum 12 month process if hearings are required).

X Ratesetting - “Ratesetting” proceedings are proceedings in which the Commission sets or investigates rates for a specifically named utility (or utilities), or establishes a mechanism that in turn sets the rates for a specifically named utility (or utilities). “Ratesetting” proceedings include complaints that challenge the reasonableness of rates or charges, past, present, or future. Other proceedings may also be categorized as ratesetting when they do not clearly fit into one category, such as railroad crossing applications (maximum 18 month process if hearings are required).

☐ Quasi-legislative - “Quasi-legislative” proceedings are proceedings that establish policy or rules (including generic ratemaking policy or rules) affecting a class of regulated entities, including those proceedings in which the Commission investigates rates or practices for an entire regulated industry or class of entities within the industry.

B. Are hearings necessary? Yes X No

If yes, identify the material disputed factual issues on which hearings should be held, and the general nature of the evidence to be introduced. Railroad crossing applications which are not controversial usually do not require hearings.

Are public witness hearings necessary?

Yes X No

Public witness hearings are set up for the purpose of getting input from the general public and any entity that will not be a party to the proceeding. Such input usually involves presenting written or oral statements to the presiding officer, not sworn testimony. Public witness statements are not subject to cross-examination.

C. Issues - List here the specific issues that need to be addressed in the proceeding.

None.

D. Schedule (Even if you checked “No” in section “B” above) should the Commission decide to hold hearings, indicate here the proposed schedule for completing the proceeding within 12 months (if categorized as adjudicatory) or 18 months (if categorized as ratesetting or quasi-legislative).

The schedule should include proposed dates for the following events as needed:

__Date____Filing Date

__Date____Comment Period (1 month from filing)

__Date____Proposed Decision (4 months from filing)

__Date____Final Decision (6 months from filing)

If hearing unexpectedly becomes necessary:

__Date____Prehearing conference

__Date____Hearing

__Date____Brief due

__Date____Submission

__Date____Proposed decision (90 days after submission)

__Date____Final decision (60 days after proposed decision is mailed)


EXHIBIT K

Certification of Service

**Dragados Flatiron Joint Venture
1775 Park Street Suite 75
Selma, CA 93662**

CERTIFICATE OF SERVICE

I, Valentin Ibarra, of Dragados Flatiron Joint Venture, on behalf of the California High-Speed Rail Authority, certify that I have this day mailed a copy of the attached Application and Exhibits in the above captioned proceeding by FedEx, or if noted, by e-mail or hand delivery, to each party named in the following service list, on this 8TH day of FEBRUARY, 2020 at Selma, California.

By: 
Valentin Ibarra
Project Engineer

Service List	
Parties:	
Bruce W. Armistead Director of Operations & Maintenance California High-Speed Rail Authority 770 L Street, Suite 620 Sacramento, CA 95814 Bruce.Armistead@hsr.ca.gov Email Only	Troy Tambay Design Build Oversight Manager California High-Speed Rail Authority 1775 Park Street, Building 75A Selma, CA 93662 Troy.Tambay@hsr.ca.gov Email Only
State:	
Anne Simon Chief Administrative Law Judge California Public Utilities Commission 505 Van Ness Avenue San Francisco, CA 94102 Anne.simon@cpuc.ca.gov Email Only	Anton Garabetian, P.E., Supervisor Rail Crossings and Engineering Branch California Public Utilities Commission 320 West 4th Street, Suite 500 Los Angeles, CA 90013 antranig.garabetian@cpuc.ca.gov Email Only
Marvin Kennix, Utilities Engineer Rail Crossings and Engineering Branch California Public Utilities Commission 180 Promenade Circle, Suite 115 Sacramento, CA 95834-2939 marvin.kennix@cpuc.ca.gov Email Only	

Information Only:	
<p>Haidar Haddadin Construction Oversight Manager California High-Speed Rail Authority 1775 Park Street, Building 75A Selma, CA 93662 Haidar.Haddadin@hsr.ca.gov Email Only</p>	<p>Valentin Ibarra Project Engineer Dragados Flatiron Joint Venture 1775 Park Street, Building 75 Selma, CA 93662 Vibarra@dfcp23.com Email Only</p>
<p>Kevin McAlister Kings County Public Works Director County of Kings 1400 W. Lacey Blvd Hanford, CA 93230 Kevin.McAlister@co.kings.ca.us Email Only</p>	<p>Johnathan Doyel City of Hanford Public Works Director 900 S 10th Ave, Hanford, CA 93230 Jdoyel@cityofhanfordca.com Email Only</p>

BEFORE THE PUBLIC UTILITIES COMMISSION OF THE STATE OF CALIFORNIA

Application of the California High-Speed Rail
Authority for Approval to Construct One
Underpass Grade-Separated Crossing at 7th
Road (MP 223.96), Under Two Proposed High-
Speed Rail Tracks As Part of the Proposed
High-Speed Rail Grade-Separated Structure,
Hanford Viaduct, Located in the County of
Kings, State of California.

Application No. _____

CPUC ENVIRONMENTAL AND SOCIAL ACTION PLAN
HANFORD VIADUCT UNDERPASS AT 7th ROAD

The Applicant, California High-Speed Rail (“Authority” or “CHSRA”) hereby seeks authority from the Commission to construct one new underpass grade-separated crossing at 7th Road under the High-Speed Rail tracks. This project is located within the Construction Package 2-3 (“CP 2-3”) in Kings County, State of California. The proposed grade-separated crossing at 7th Road will cross under the CHSRA mainlines, respectively, and are part of the proposed California High-Speed Train System (CHSTS).

In support of this Application, and that the noted project is consistent with the CPUC’s Environmental and Social Justice (ESJ) Action Plan, and the Commission’s vision to advance equity in its programs and policies for environmental and social justice in this project’s communities, the CHSRA respectfully submits the ESJ plan herein. As an overview, this grade-separated crossing in the CP 2-3 project is to provide safety for motorists and pedestrians. The grade-separated crossing at 7th Road will allow for safe and simultaneous movement of roadway and rail modes of travel. All CHSRA structures are in compliance with AREMA Standards throughout the project.

Goal 1. Consistently integrated equity and access considerations throughout CPUC proceedings and other efforts.

The Application and project are consistent with the advancement of this goal. In support of this Application, the California High-Speed Rail Authority (Applicant) submitted the *Final Environmental Impact Report / Environmental Impact Statement for the Fresno to Bakersfield Section of the California High-Speed Train Project (Final EIR / EIS)* on an archival CD-ROM/DVD and six copies on standard CD-ROM/DVD to the Docket Office for physical filing as Exhibit I to application A1709017. The approved document included in DVD format contains the Record of Decision (ROD), and the Memorandum of Agreement between the Federal Railroad Administration (FRA), CHSRA, and the Surface Transportation Board (STB).

Goal 2: Increase investment in clean energy resources to benefit ESJ communities, especially to improve local air qualities and public health.

The Application and project are consistent with the advancement of this goal. The CHSTS will be fully electric with overhead contact system (OSC) providing consistent power to the train. This style has significant environmental benefits in transforming our transportation system from a fossil-fuel dependent system to one moving toward electrification including a green energy strategy. This solution will reduce traffic congestion and decrease fossil fuel consumption, which will effectively improve air quality. Studies found that the average annual GHG emission savings of the CHSRA system is 1.9 million metric tons of carbon dioxide, and is projected to be the equivalent of taking 400,000 passenger vehicles off the road, and saving 213 million gallons of gasoline every year.

Goal 3: Strive to improve access to high quality water, communications, and transportation services for ESJ communities.

The Application and project are consistent with the advancement of this goal. As we proceed with advancement for transportation, we are now connecting vital areas in California to be tightly linked networks for businesses, education, and travelers alike. CHSRA will be creating a lifelong sustainable economic impact by connecting the Bay Area, Central Valley, and Los Angeles regions by having them codependently working together. CHSRA will reduce traffic congestion and improve

safety along California by decreasing daily commutes with highly efficient renewable energy transportation. In addition, with a decline of vehicles on the road, road maintenance will reduce and provide a solution to the existing transportation safety challenges that road maintenance crews face every day.

Goal 4: Increase climate resiliency in ESJ communities.

The Application and project advance this goal. As described in Goal 2, the project will provide significant environmental benefits that will increase climate resiliency throughout the state. During operations throughout the corridor, we are having contractors use clean-burning fuels equipment to improve air quality. This reduces the daily amount of emissions the project creates from earthmoving and concrete construction equipment.

Goal 5: Enhance outreach and public participation opportunities for ESJ communities to meaningfully participate in the CPUC's decision-making process and benefit from CPUC programs.

As set forth above regarding Goal 1, this goal is advanced by the Applicant and project, as there have been regular and robust opportunities for the community, stakeholders along the corridor which include government agencies, and railroads in the development of the project.

Goal 6: Enhance enforcement to ensure safety and consumer protection for ESJ communities.

The Application and project are consistent with the advancement of this goal. For the protection of the general public and CHSTS passengers, the CHSTS will be a closed corridor with a continuous 8 foot high welded wire mesh Access Restricted (AR) fence or barrier to prevent unauthorized access to CHSRA right-of-way. In addition to the AR fence or barrier walls, intrusion monitoring and detection are proposed.

Goal 7: Promote economic and workforce development opportunities in ESJ communities.

The Application and project are consistent with the advancement of this goal. The project will have significant positive economic impact in California by providing jobs to local unions and small

businesses supporting the initial construction phase. In addition, with businesses providing material for the project, it has generated sale tax revenue for local counties. From July 2006 to June 2019, 44,700 jobs have been created, \$3.17B labor income, and \$8.3B economic output.

Goal 8: Improve training and staff development related to ESJ issues within the CPUC's jurisdiction.

Applicant defers to staff in relation to this topic as it is an internal Commission goal.

Goal 9: Monitor to CPUC's ESJ efforts to evaluate how they are achieving their objectives

Applicant defers to staff in relation to this topic as it is an internal Commission goal.

Respectfully submitted,

A handwritten signature in black ink, appearing to read "peLnt", is centered within a black rectangular box. A horizontal line extends from the left side of the box, and another extends from the right side.

Bruce Armistead
California High-Speed Rail Authority
770 L Street, Suite 620
Sacramento, CA 95814